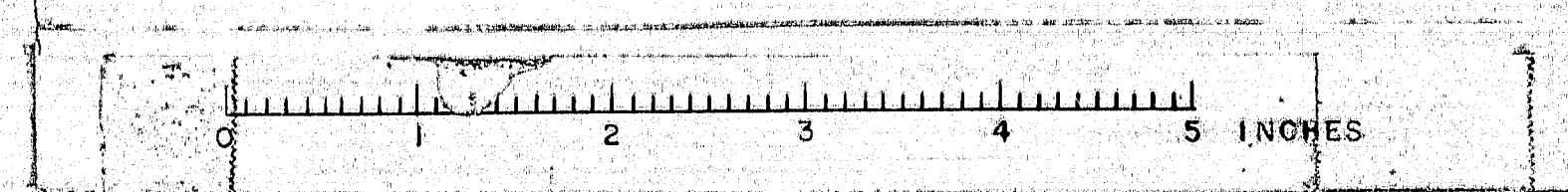


1- BREWER SHOP - 6-19-63  
3- CUSTOMER 6-19-63  
2- BRIDGE DIVISION - 6-19-63

SHOP CONNECTIONS: *WELDED*  
FIELD CONNECTIONS: *—*  
HOLES: *AS NOTED*  
PAINT: *PER ME. STATE SPECS,*  
*RED LEAD & AS NOTED*

BEARING DETAILS	
Bancroft & Martin Inc. South Portland 7, Maine	
I- 95 OVER SOMERSET AVE PITTSFIELD, MAINE	
CUSTOMER	CIANCHETTE BROS.
DESIGNER	ME. S. H. C.
ORDER NO. VERBAL	DWG. NO. B-63-4954



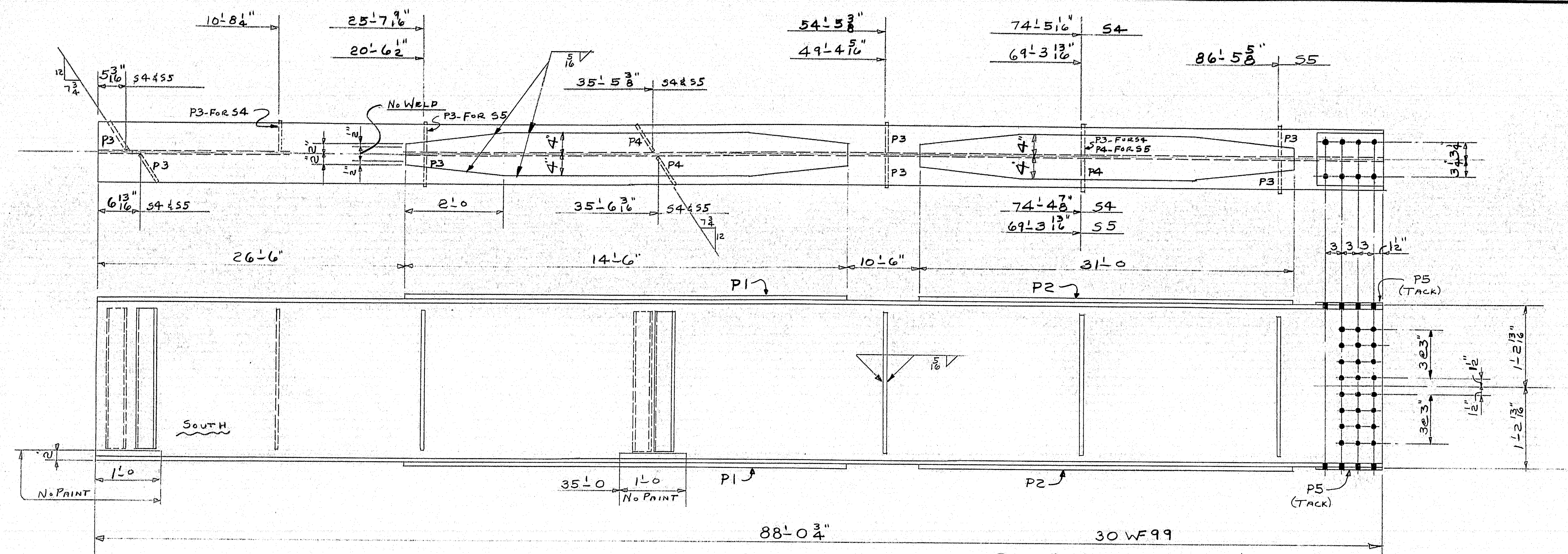






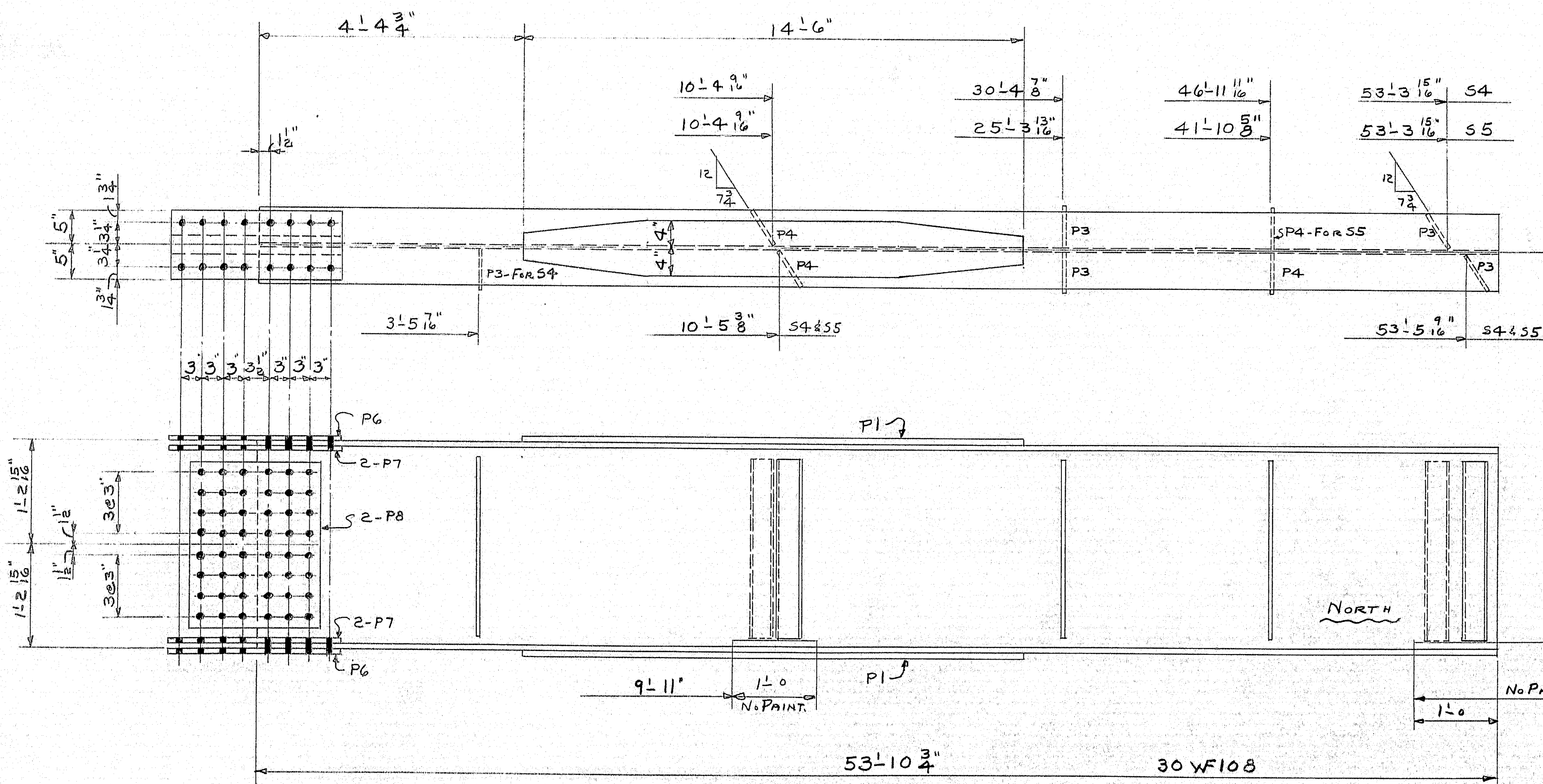






ONE - S4N  
ONE - S4S  
ONE - S5N  
ONE - S5S

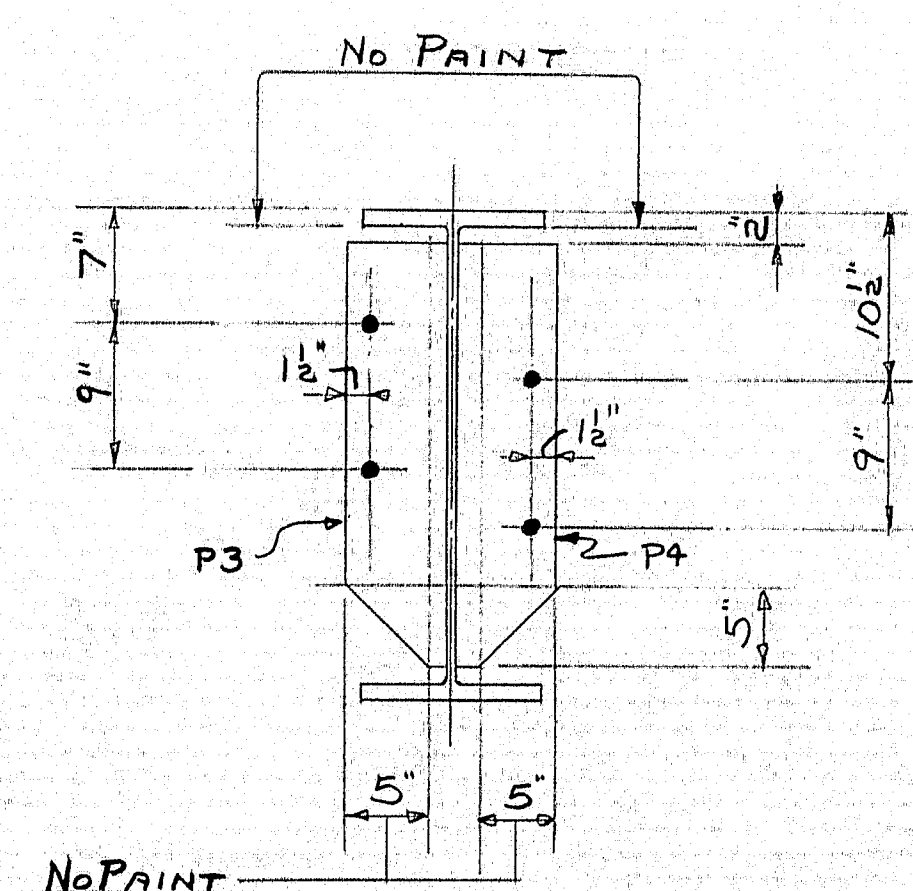
PLACE ANY NATURAL CAMBER UP



ONE - S4N  
ONE - S4S  
ONE - S5N  
ONE - S5S

PLACE ANY NATURAL CAMBER UP

NOTE:  
MATCH ENDS OF STRINGERS THAT  
HAVE THE SAME PIECE MARK.  
SEE DWG. B63-49-E1 FOR SPLICE DRILLING.

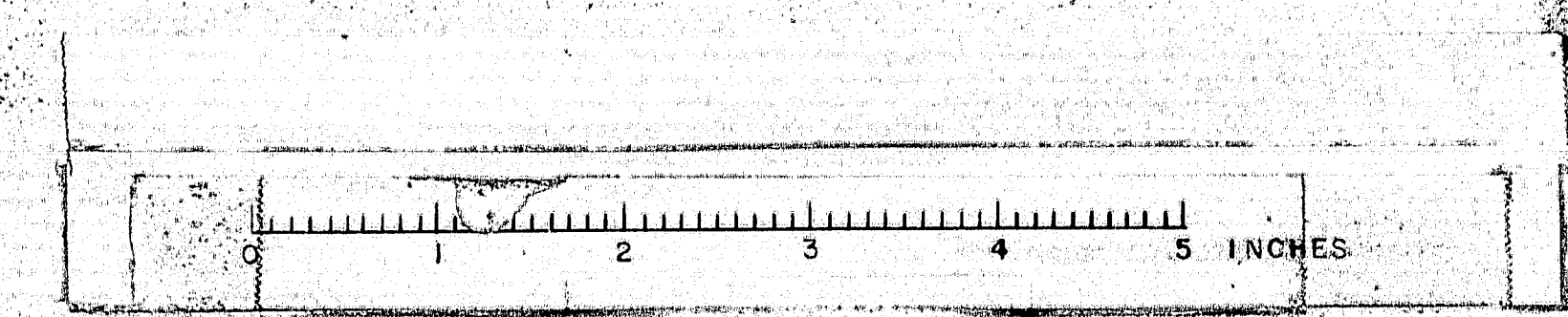


SHOP CONNECTIONS: WELD (ETO-ELECTRODES)  
FIELD CONNECTIONS: WELD & 3/8 H.S. BOLTS  
HOLES: 1/8" Ø  
PAINT: STATE OF MAINE SPEC'S

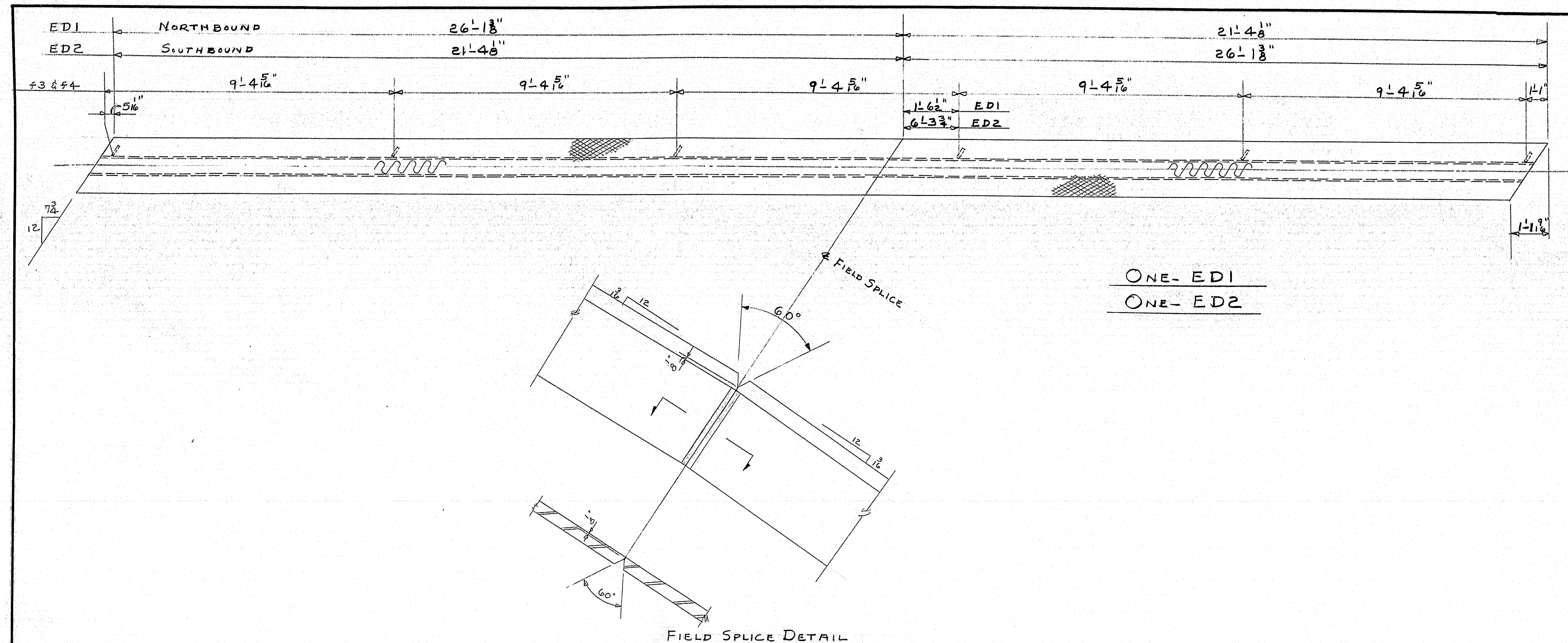
SHIP		BILL OF MATERIAL			DWG. B63-49-54	
MARK	NO.	MARK	SHAPE	LENGTH	WT.	REMARKS
S4N	1		30WF99	88'0 3/4"		A36
S4S	1		D.	88'0 3/4"		
S5N	1		D.	88'0 3/4"		
S5S	1		D.	88'0 3/4"		
S4N	1		30WF108	53'10 3/4"		
S4S	1		D.	53'10 3/4"		
S5N	1		D.	53'10 3/4"		
S5S	1		D.	53'10 3/4"		
	16	P1	R8x5	14'6"		
	8	P2	R8x1 1/2	31'0"		
	8	P5	R10x3/8	1'0"		
	8	P6	R10x3/8	2'0 1/2"		
	16	P7	R4x1/2	2'0 1/2"		
	8	P8	R18 1/2 x 2	2'0"		
	52	P3	R6x3/8	2'2"		A7
	28	P4	D.	2'2"		A7
STRUCTURAL STEEL IS ITEM 702-103						

APP'D AS NOTED 6-12-63		STRINGERS	
PRINT ISSUE		Bancroft & Martin Rolling Mills Company Brewer, Maine	
2	SHG.	6-19-63	INTERSTATE 95 OVER SUMMERSET AVE.
5	SHOP	6-18-63	PITTSFIELD MAINE
3	DIST.	6-18-63	
2	FIA	5-29-63	CUSTOMER: CLANCHETTE BROS.
DRAWN	4-15-63	D.C.	DESIGNER: STATE HIGHWAY COMM.
REVISION			
REVISION			
REVISION			
ORDER VERBAL		DWG. B63-49-54	

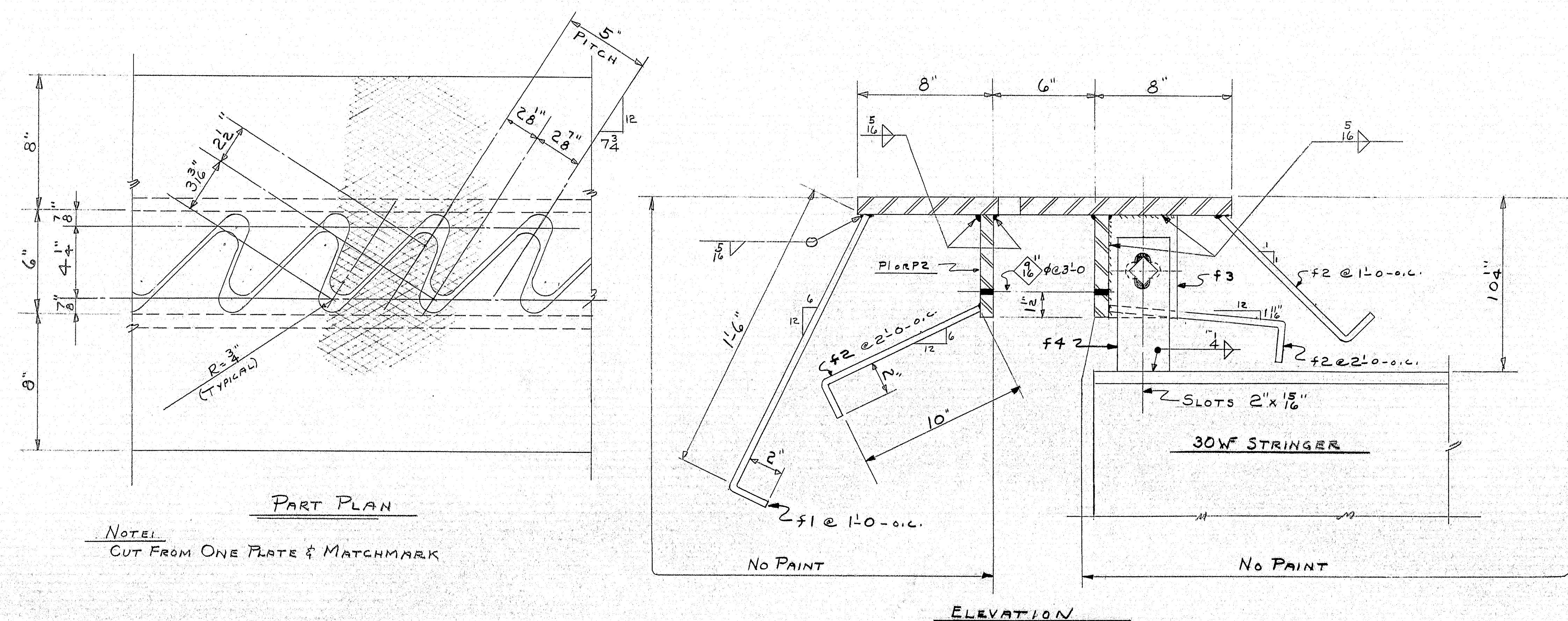
94-105







SHIP		BILL OF MATERIAL				DWG. B63-49-55
MARK	NO.	MARK	SHAPE	LENGTH	WT.	REMARKS
EDIR	1		R 21x1	27 3		CHK'D RE A7
EDIL	1		D°	22 5 3/4		D°
ED2R	1		D°	22 5 3/4		D°
ED2L	1		D°	27 3		D°
	4	P1	R 6x3/4	26 1 3/8		
	4	P2	D°	21 4 5/8		
	96	f1	BAR 2x3/8	1 8		
	192	f2	D°	1 0		
	12	f3	BAR 4x3/8	0 6		
	12	f4	BAR 3x3/8	0 8		
	12	SHOP	3/8 H.S. BOLT	0 2 1/2		
	24	SHOP	3/8 H.S. WASHERS			
STRUCTURAL STEEL IS ITEM						702-103



NOTE:  
CUT FROM ONE PLATE & MATCHMARK

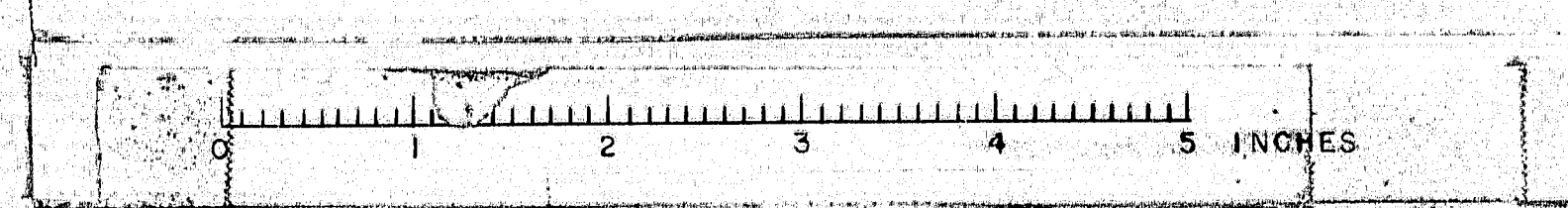
SHOP CONNECTIONS: WELD (E70 ELECTRODES)  
FIELD CONNECTIONS: WELD  
HOLES: AS NOTED  
PAINT: STATE OF MAINE SPEC'S.

APP'D AS NOTED 6-12-63

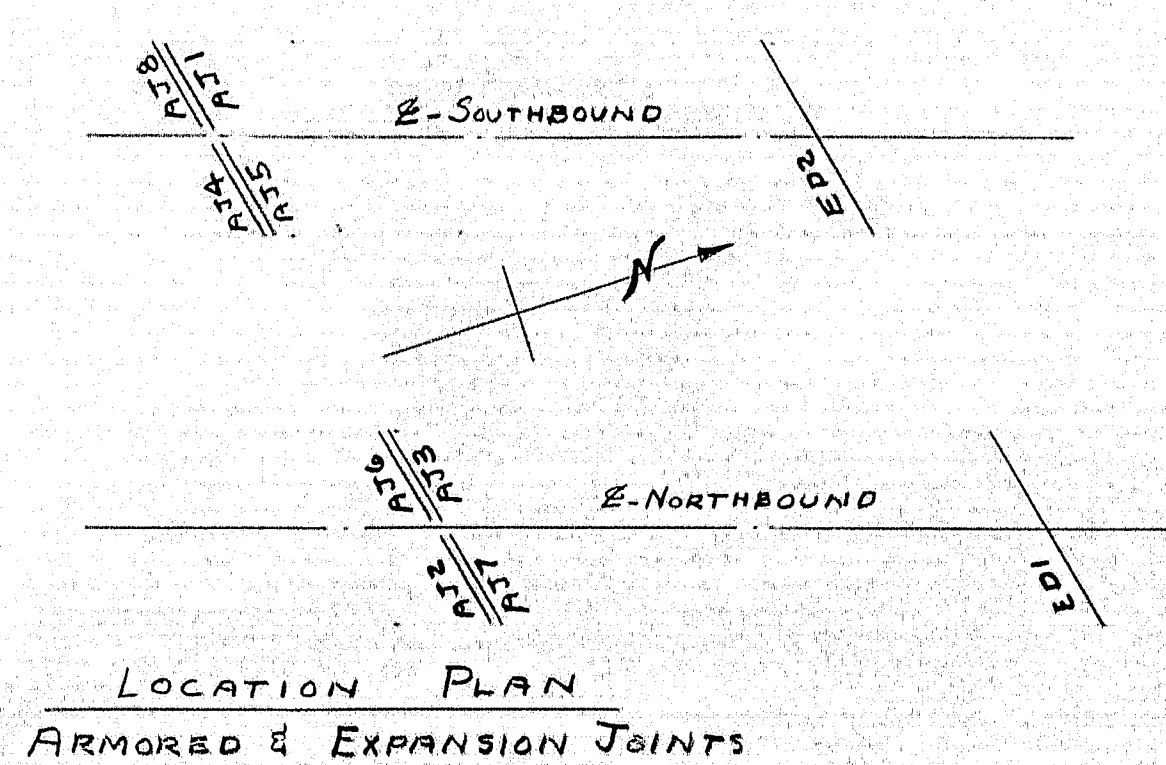
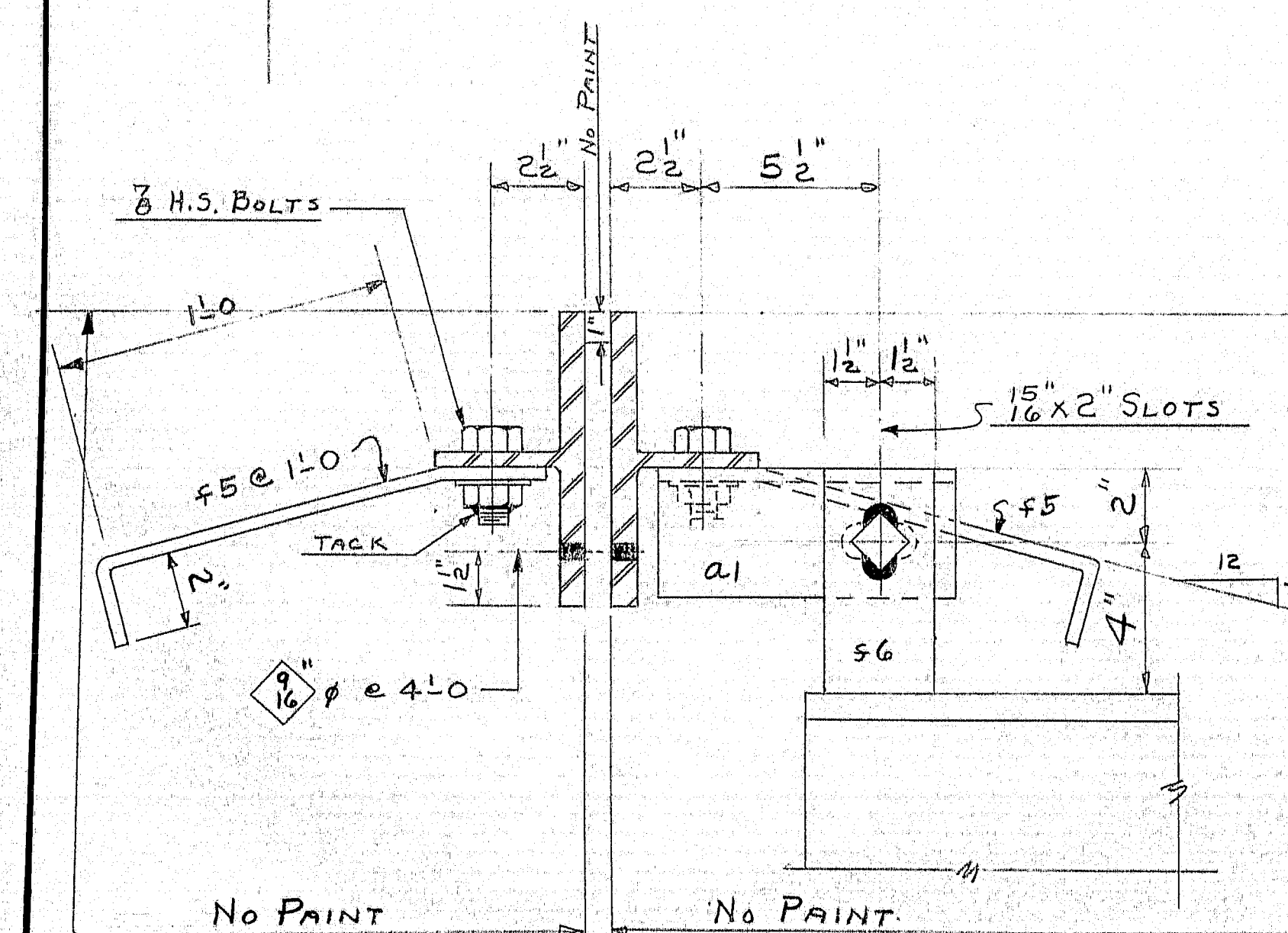
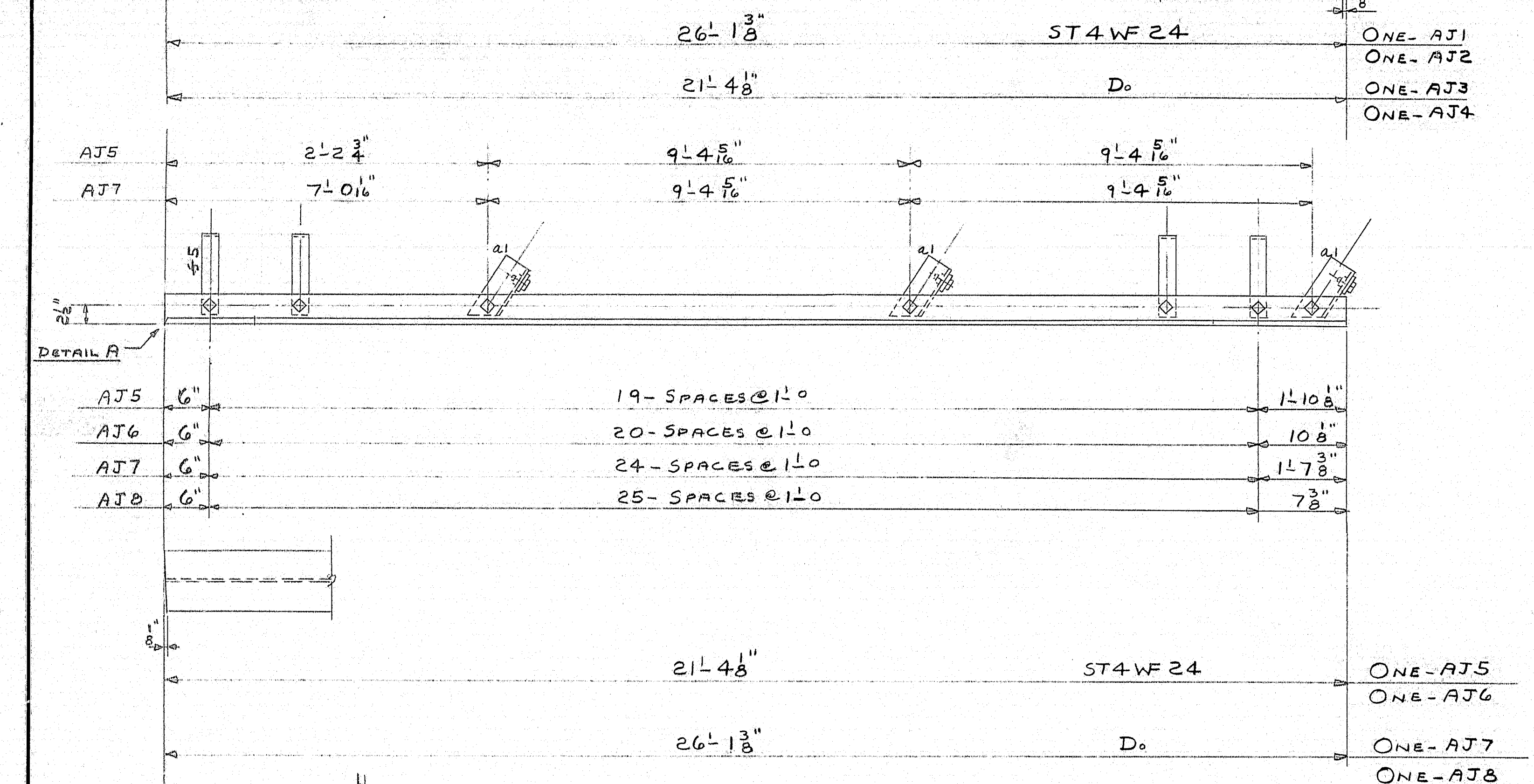
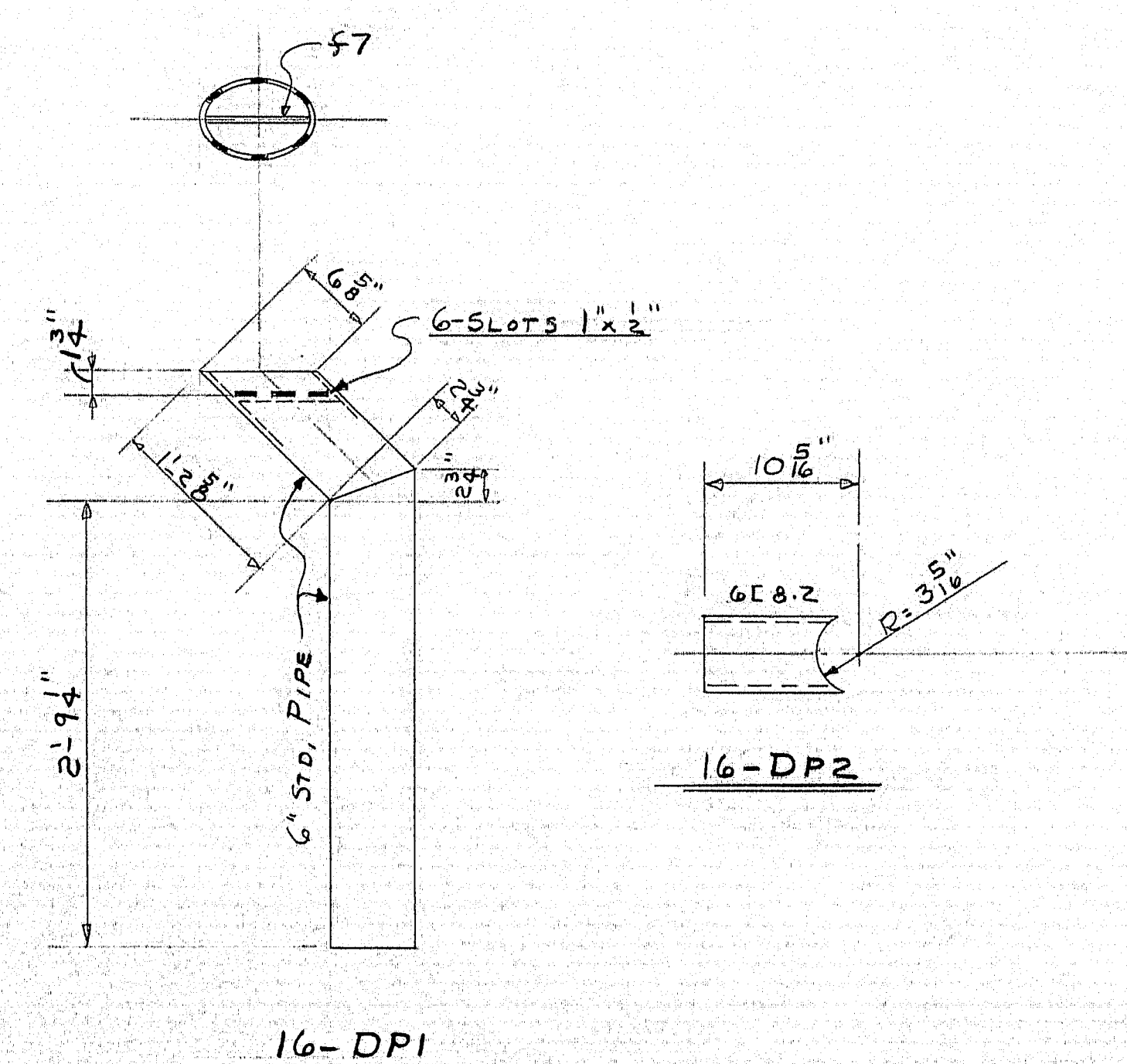
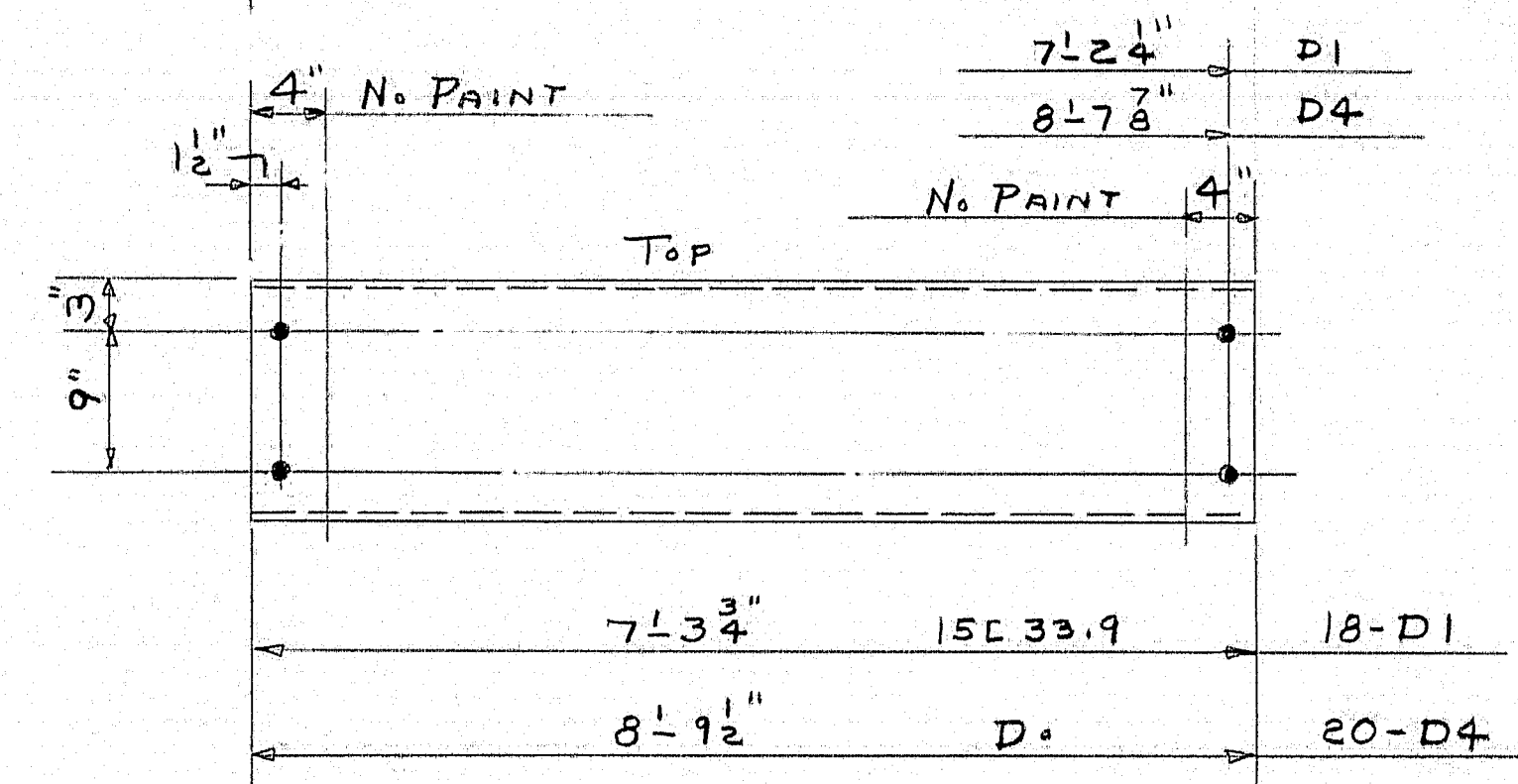
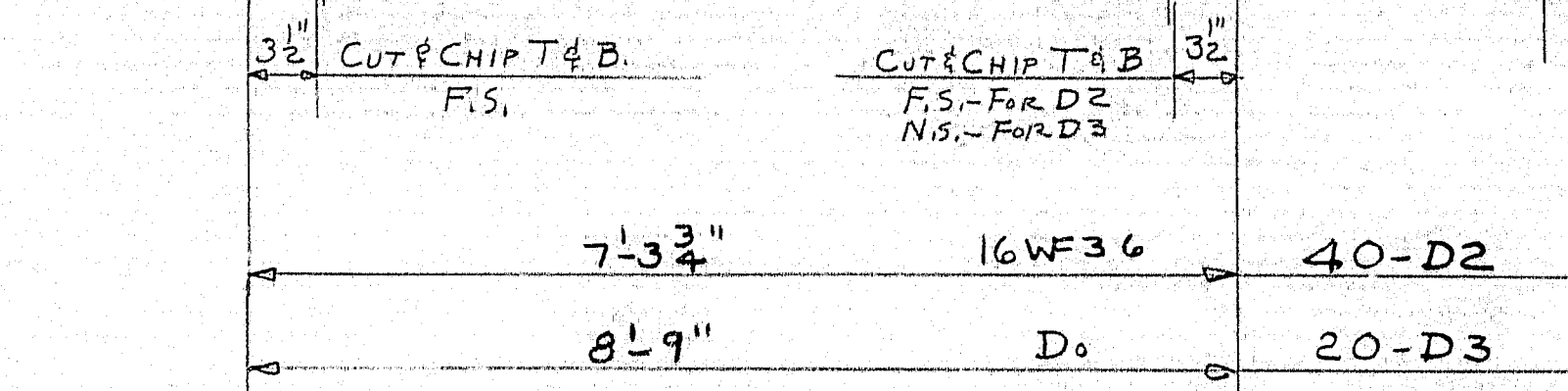
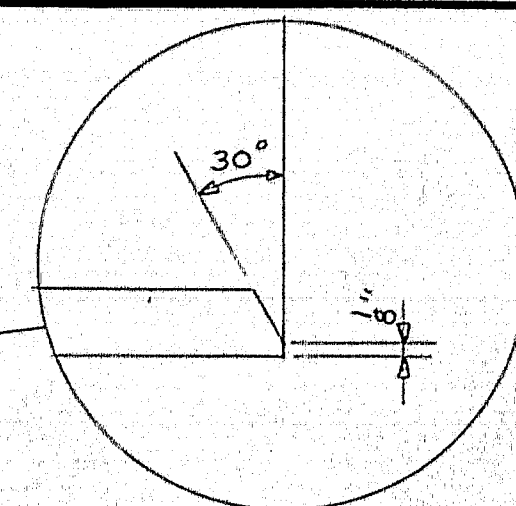
EXPANSION DAMS AT ABUT #2 NORTH & SOUTH

PRINT ISSUE		Bancroft & Martin Rolling Mills Company	
		Brewer, Maine	
2	SHC.	6-19-63	INTERSTATE #95 OVER SOMERSET AVE.
5	SHOP	6-18-63	PITTSFIELD MAINE
3	DIST.	6-18-63	
2	F/A	5-29-63	
DRAWN	4-15-63 D.C.	CUSTOMER CLANCHATTER BA's	
REVISION		DESIGNER STATE HIGHWAY COMM.	
REVISION		ORDER VERBAL	
REVISION		DWG. B63-49-55	

94-106





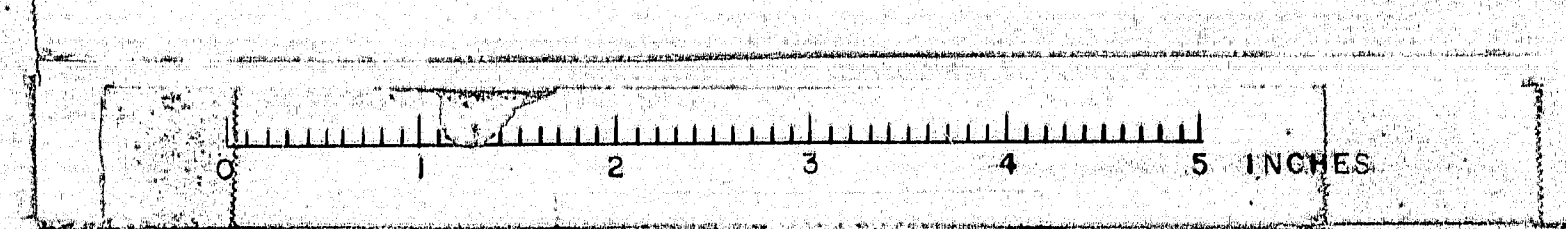
[illegible]

SHOP CONNECTIONS: WELD (E70-ELECTRODES)  
FIELD CONNECTIONS: WELD & B.H.S, BOLTS  
HOLES:  $\frac{15}{16}$ "  $\phi$  UNLESS NOTED  
PAINT: STATE OF MAINE SPEC'S.

APP'D AS NOTED 6-12-63

ARMORED JOINTS, DIAPHRAGMS & DRAINS			
PRINT ISSUE		<i>Bancroft &amp; Martin Rolling Mills Company</i> <i>Brewer, Maine</i>	
2	SHC 6-19-63		INTERSTATE 95 OVER SOMERSET AVE PITTSFIELD MAINE
5	SHOP 6-18-63		
3	DIST 6-18-63		
2	FIA 5-29-63		
DRAWN	4-16-63 D.C.	CUSTOMER <u>CLANCHETTE BROS.</u>	
REVISION		DESIGNER <u>STATE HIGHWAY COMM.</u>	
REVISION		ORDER <u>VERBAL</u> DWG. <u>B63-49-56</u>	
REVISION			

94-107





## SPECIFICATIONS

DESIGN - A.A.S.H.O. STANDARD SPECIFICATIONS FOR HIGHWAY BRIDGES 1961.

CONTRACT - STATE OF MAINE, STATE HIGHWAY COMMISSION, STANDARD SPECIFICATIONS HIGHWAYS AND BRIDGES, REVISION OF JANUARY 1956, AND SUPPLEMENTAL SPECIFICATIONS.

## LIVE LOADING

H20-S16-44 AS MODIFIED FOR INTERSTATE HIGHWAYS.

## ALLOWABLE LOADING

STRUCTURAL STEEL A.S.T.M. DESIGNATION A 36 -  $f_y = 20,000$  p.s.i.  
REINFORCING STEEL, INTERMEDIATE GRADE -  $f_s = 20,000$  p.s.i.  
CONCRETE ( $n = 10$ )  $f_c = 1200$  p.s.i.  
STRUCTURAL STEEL A.S.T.M. DESIGNATION A 7 -  $f_y = 18,000$  p.s.i.

## CONCRETE CLASSIFICATION

ALL CONCRETE -----, CLASS "A"  
EXCEPT CONCRETE FILL UNDER PIER NO. 2-N.B. -----, CLASS "B"

## ELEVATIONS

ELEVATIONS ARE BASED ON B.M.# 22,  
STA. 2114+25 N.B., OFF 115' LT.,  
VERTICAL SPIKE IN 30" MAPLE,  
ELEV. 223.88.

## ESTIMATE OF BRIDGE QUANTITIES

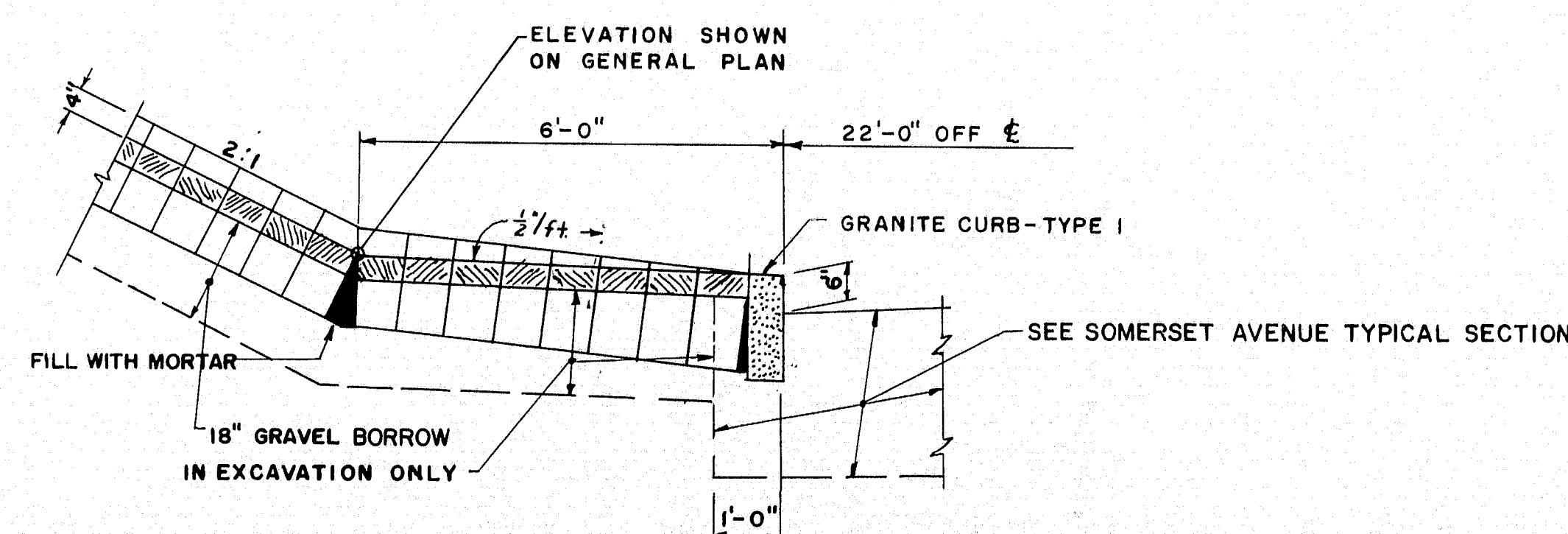
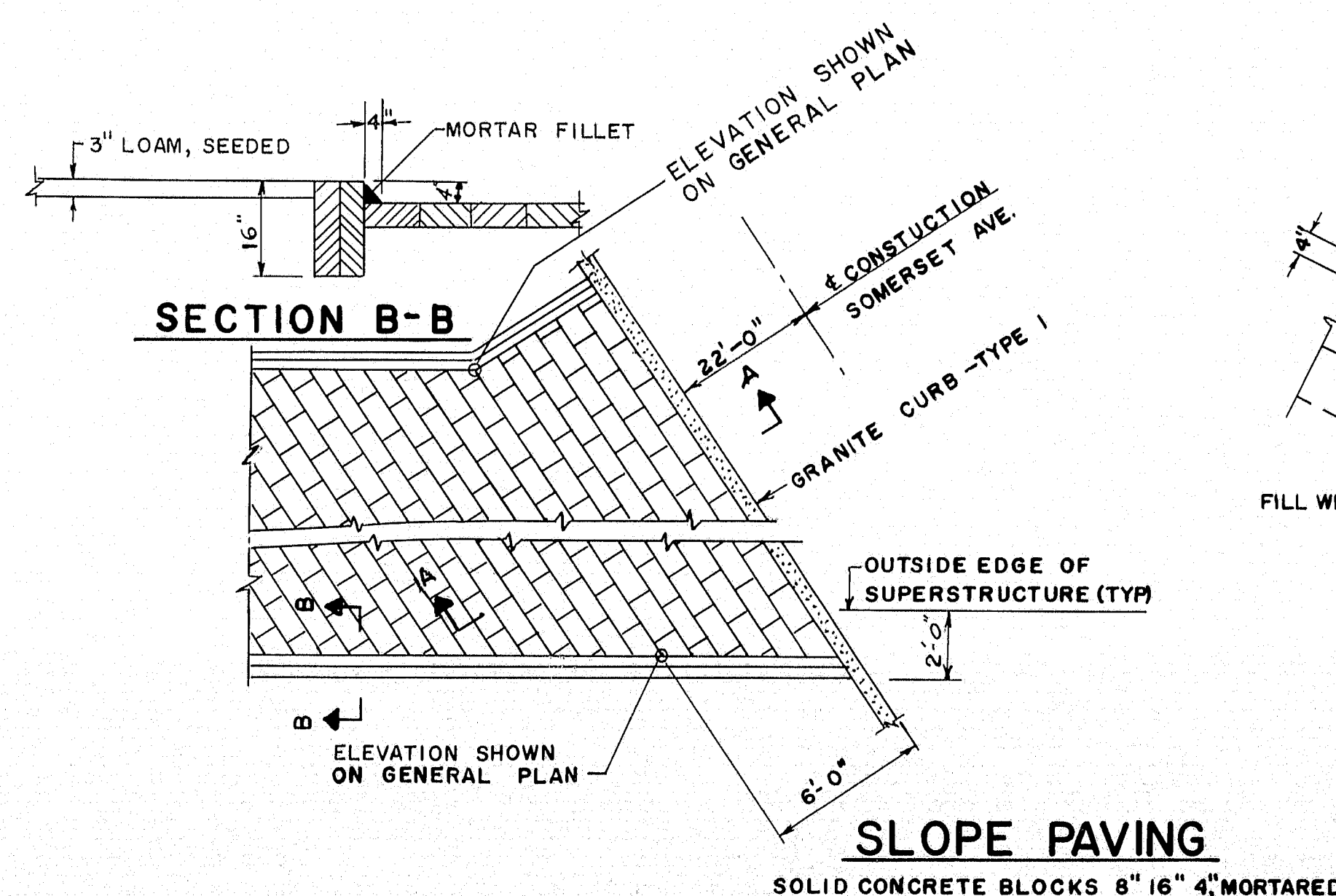
ITEM	UNIT	QUANTITY
STRUCTURAL EARTH EXCAVATION - ABUTMENTS & RETAINING WALLS	C.Y.	500
STRUCTURAL EARTH EXCAVATION - PIERS	C.Y.	800
STRUCTURAL ROCK EXCAVATION - PIERS	C.Y.	5
GRAVEL BORROW	C.Y.	700
SPECIAL GRAVEL BORROW - IN PLACE MEASURE	C.Y.	180
PORTLAND CEMENT CONCRETE - ABUTMENTS & RETAINING WALLS	C.Y.	500
PORTLAND CEMENT CONCRETE - PIERS	C.Y.	265
PORTLAND CEMENT CONCRETE - ROADWAY AND SIDEWALK SLABS ON STEEL BRIDGES	C.Y.	348
PORTLAND CEMENT	BBL.	1670
STRUCTURAL STEEL - FABRICATED & DELIVERED	L.S.	LUMP SUM
STRUCTURAL STEEL - ERECTION	L.S.	LUMP SUM
STRUCTURAL STEEL - FIELD PAINTING	L.S.	LUMP SUM
STEEL RAIL - ALTERNATE "B"	L.F.	653
REINFORCING STEEL - DELIVERED	L.B.	138,700
REINFORCING STEEL - PLACING	L.B.	138,700
FRENCH DRAINS	C.Y.	180
ALUMINUM RAIL - ALTERNATE "A"	L.F.	653
SLOPE PAVING	S.Y.	925
GRANITE BRIDGE CURB	L.F.	680
EPOXY RESIN SURFACE SEALANT	S.Y.	170
* BITUMINOUS CONCRETE SURFACE COURSE - TYPE "A"	TON	140
* MEMBRANE WATERPROOFING	S.Y.	1272

ESTIMATED QUANTITY OF STRUCTURAL STEEL INCLUDING PIPE DRAINS = 265,000 LBS.

\* NOT PART OF THIS CONTRACT

## INDEX OF SHEETS

SHEET NO.	TITLE
1.	INDEX, QUANTITIES, SPECIFICATIONS, & SLOPE PAVING
2.	GENERAL PLAN
3.	FOUNDATION SURVEY
4.	BORING DETAILS
5.	ABUTMENT NO. 1 - SOUTHBOUND
6.	ABUTMENT NO. 2 - SOUTHBOUND
7.	ABUTMENT NO. 1 - NORTHBOUND
8.	ABUTMENT NO. 2 - NORTHBOUND
9.	PIERS
10.	STRUCTURAL STEEL - FRAMING PLAN
11.	STRUCTURAL STEEL DETAILS - 1
12.	STRUCTURAL STEEL DETAILS - 2
13.	BLOCKING DETAILS
14.	SUPERSTRUCTURE - SPANS 1 & 2
15.	SUPERSTRUCTURE - SPANS 2 & 3
16.	REINFORCING STEEL SCHEDULE
BD 102-62	STANDARD DETAILS - BRIDGE RAIL
BD 103-62	STANDARD DETAILS - BEAM SPLICES



## SECTION A-A

THE 18" GRAVEL BORROW UNDER THE SLOPE PAVING MAY BE REDUCED OR OMITTED, IF IN THE OPINION OF THE ENGINEER THE EXISTING MATERIAL IS SUITABLE. PAYMENT FOR EXCAVATION FOR GRAVEL BORROW UNDER SLOPE PAVING TO BE MADE UNDER ITEM 204-14, STRUCTURAL EARTH EXCAVATION-PIERS, OR ITEM 204-15, STRUCTURAL ROCK EXCAVATION-PIERS.

DESIGN - CDH  
TRACE - B.S.H.  
CHECK - E.E.L.

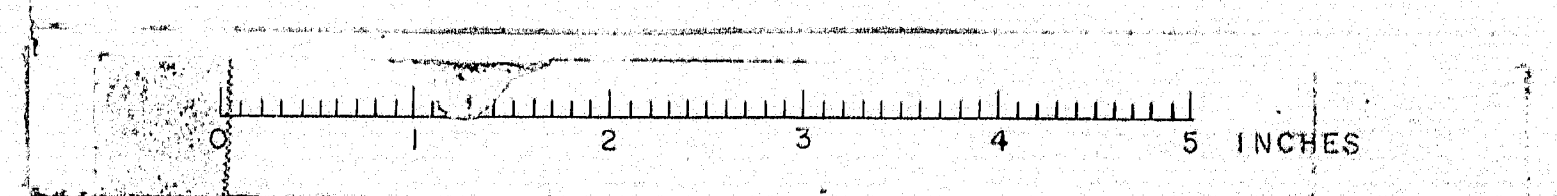
BRIDGE NO.  
SURVEY -  
PLOT -

STATE HIGHWAY COMMISSION  
BRIDGE DIVISION

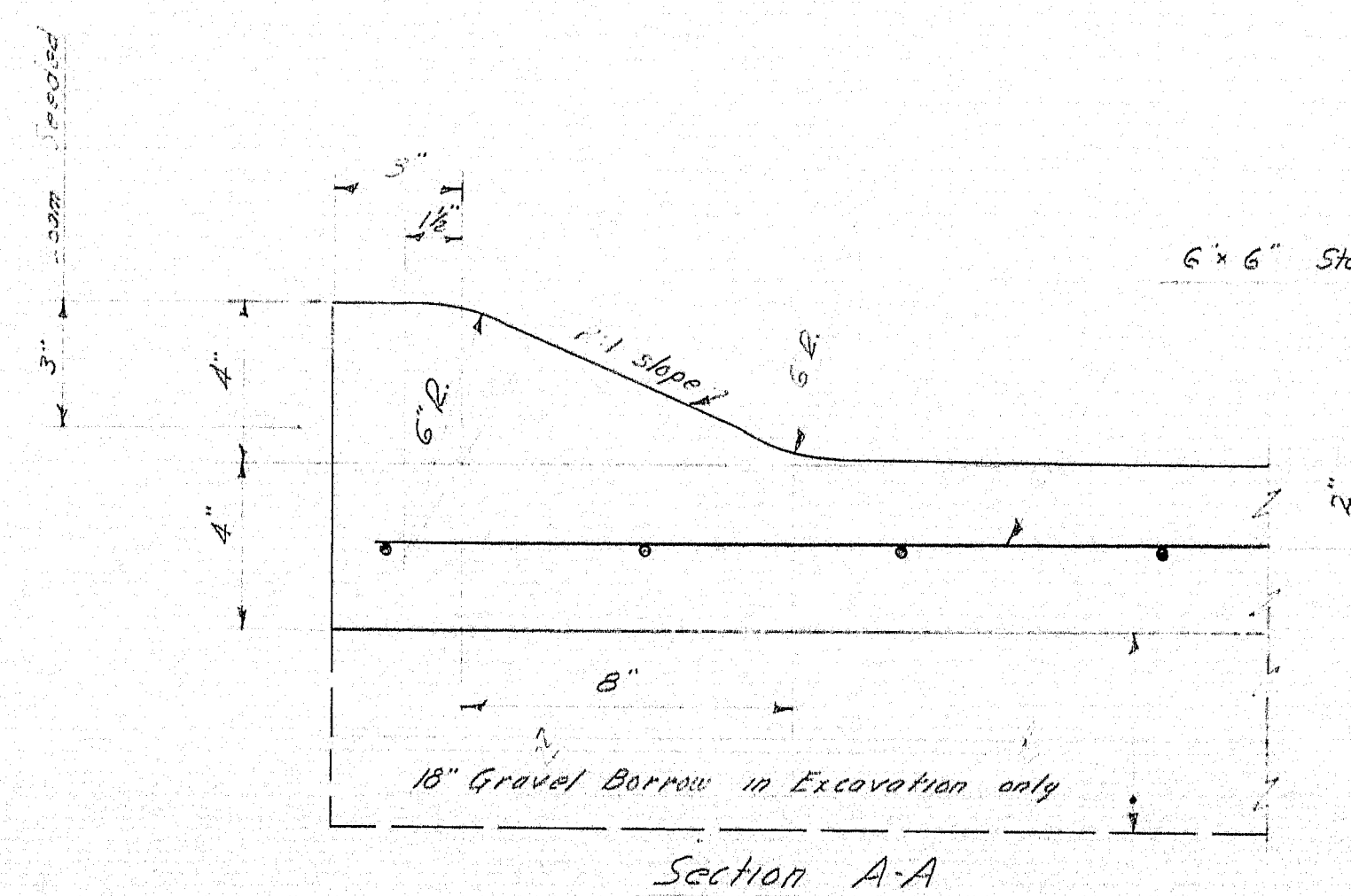
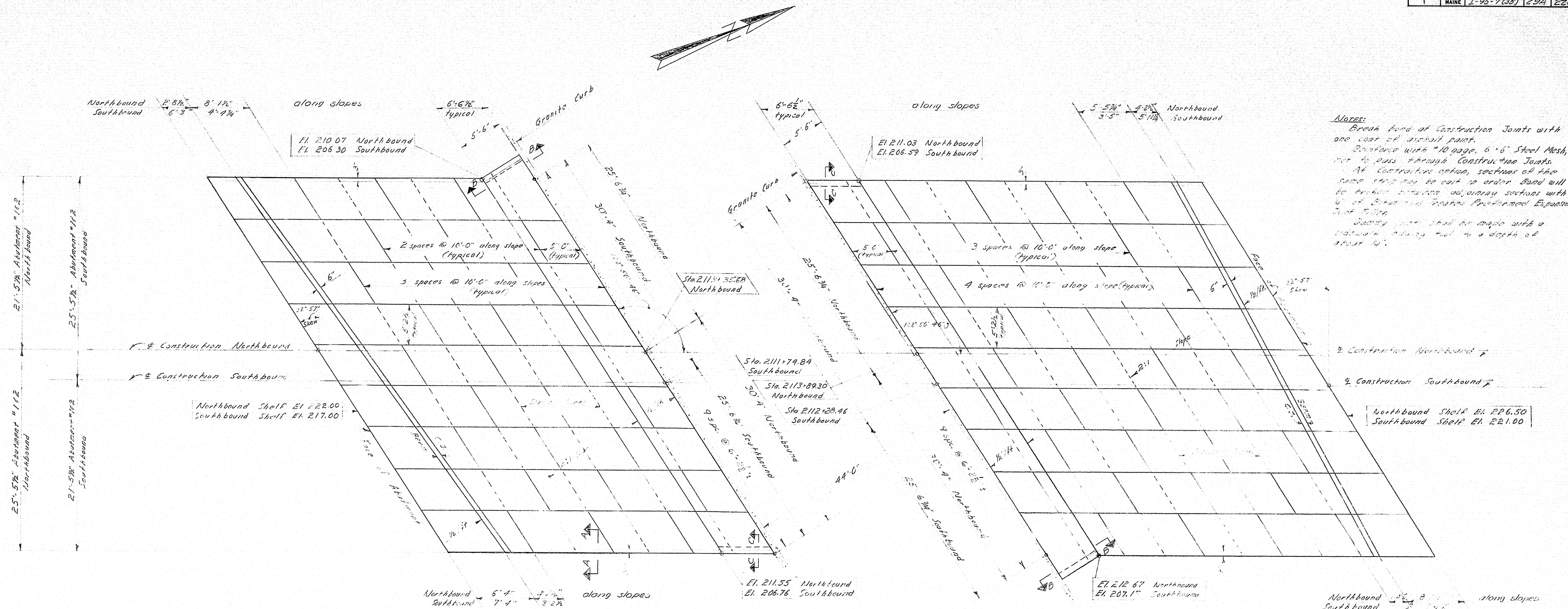
INTERSTATE 95  
OVER  
SOMERSET AVENUE  
IN THE TOWN OF  
PITTSFIELD  
SOMERSET COUNTY

INDEX, QUANTITIES, SPECIFICATIONS, SLOPE PAVING

SHEET 1 OF 16 AUGUSTA, MAINE FEB. 1963

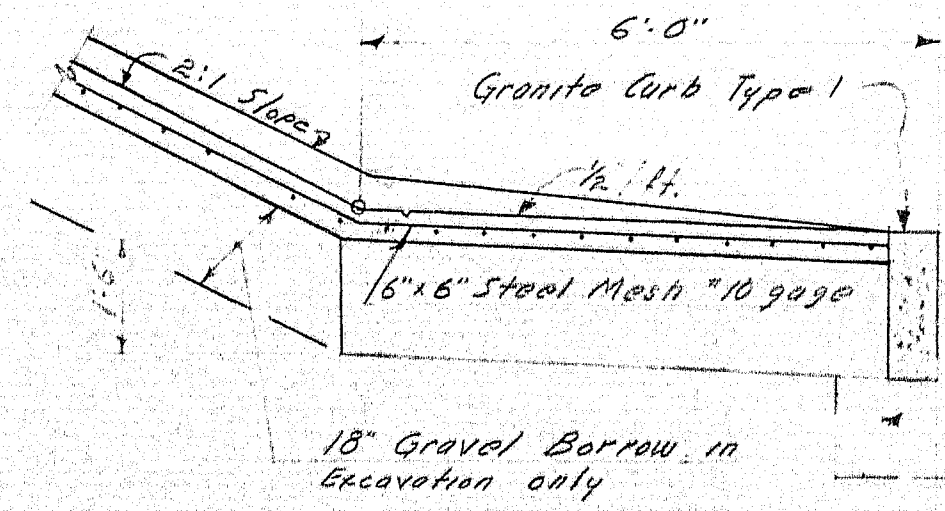




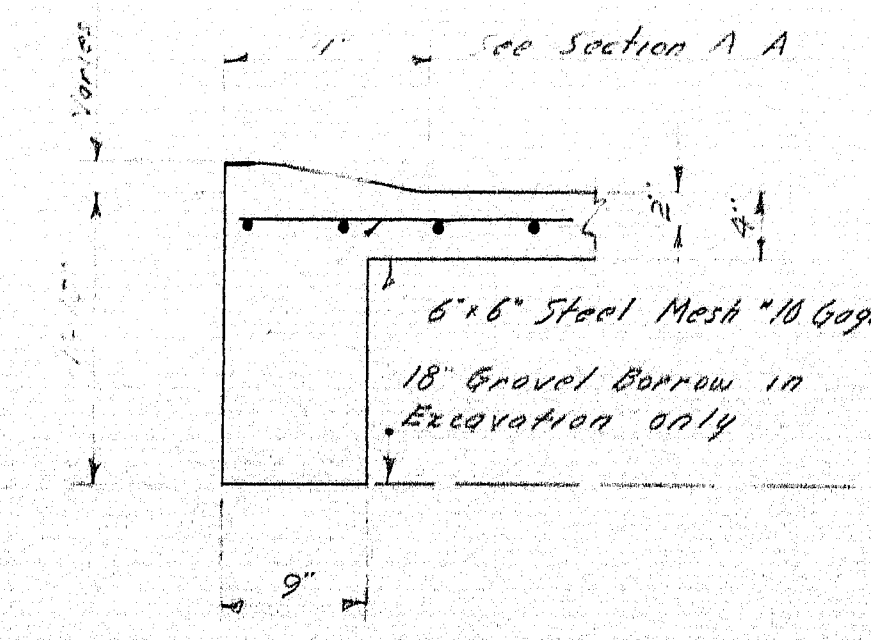


Slope Paving to Abutment No. 1

**PLAN**



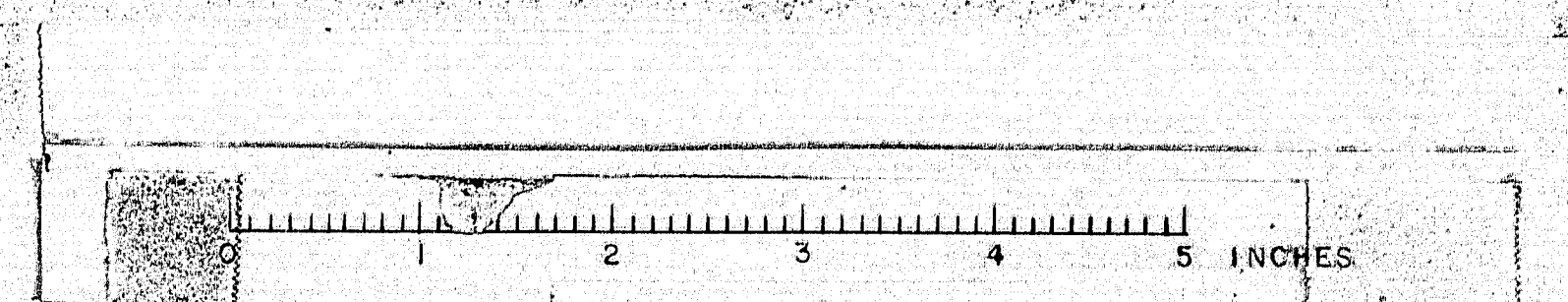
Slope Paving to Abutment No. 2



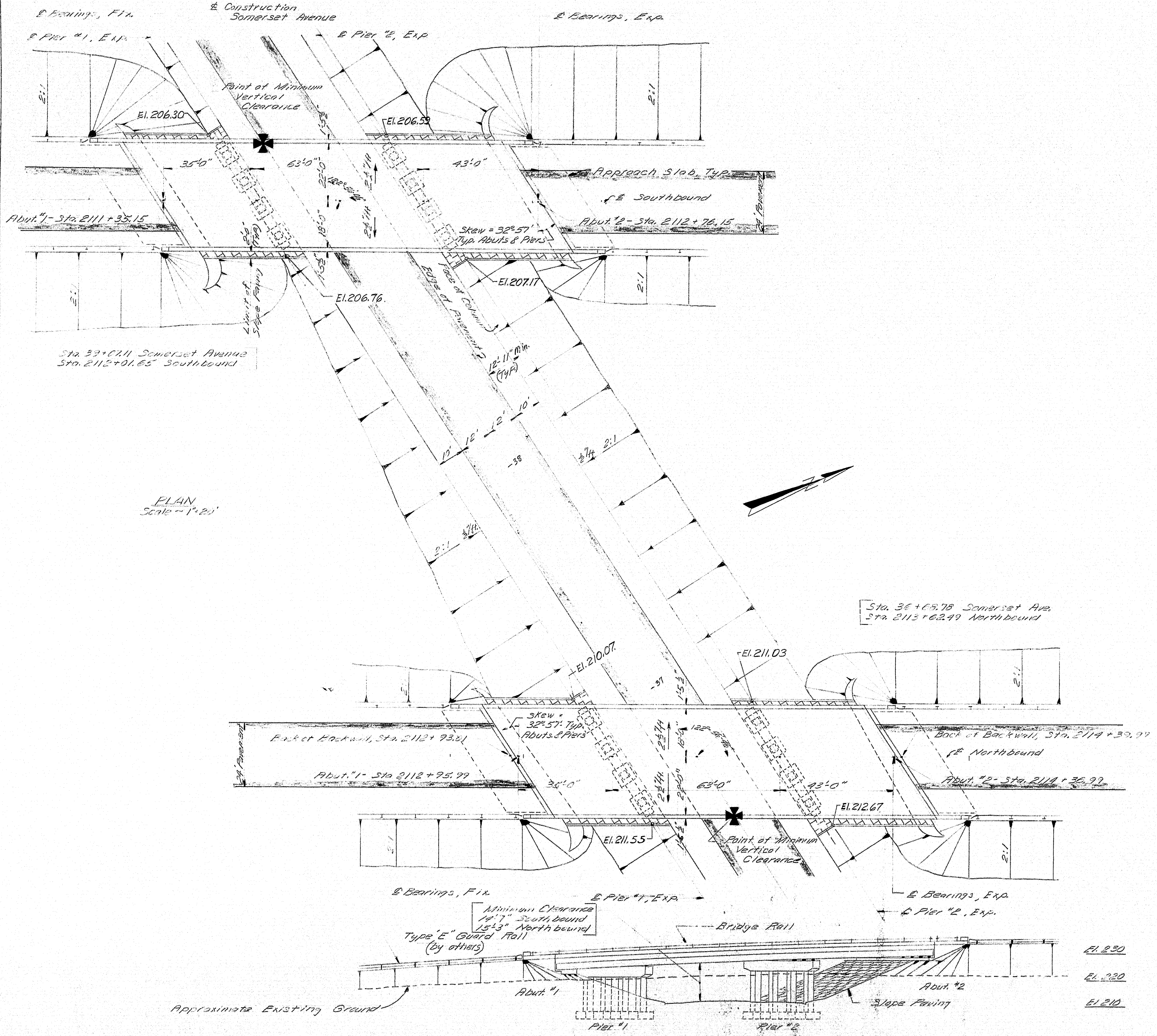
NOTES:

The 18" Gravel Barrow under the slope paving may be reduced or omitted, if in the opinion of the Engineer the existing material is suitable. Payment for Excavation for Gravel Barrow under Slope Paving to be made under Item 204-14, Structural Earth Excavation - Piers, or Item 204-15, Structural Rock Excavation - Piers.

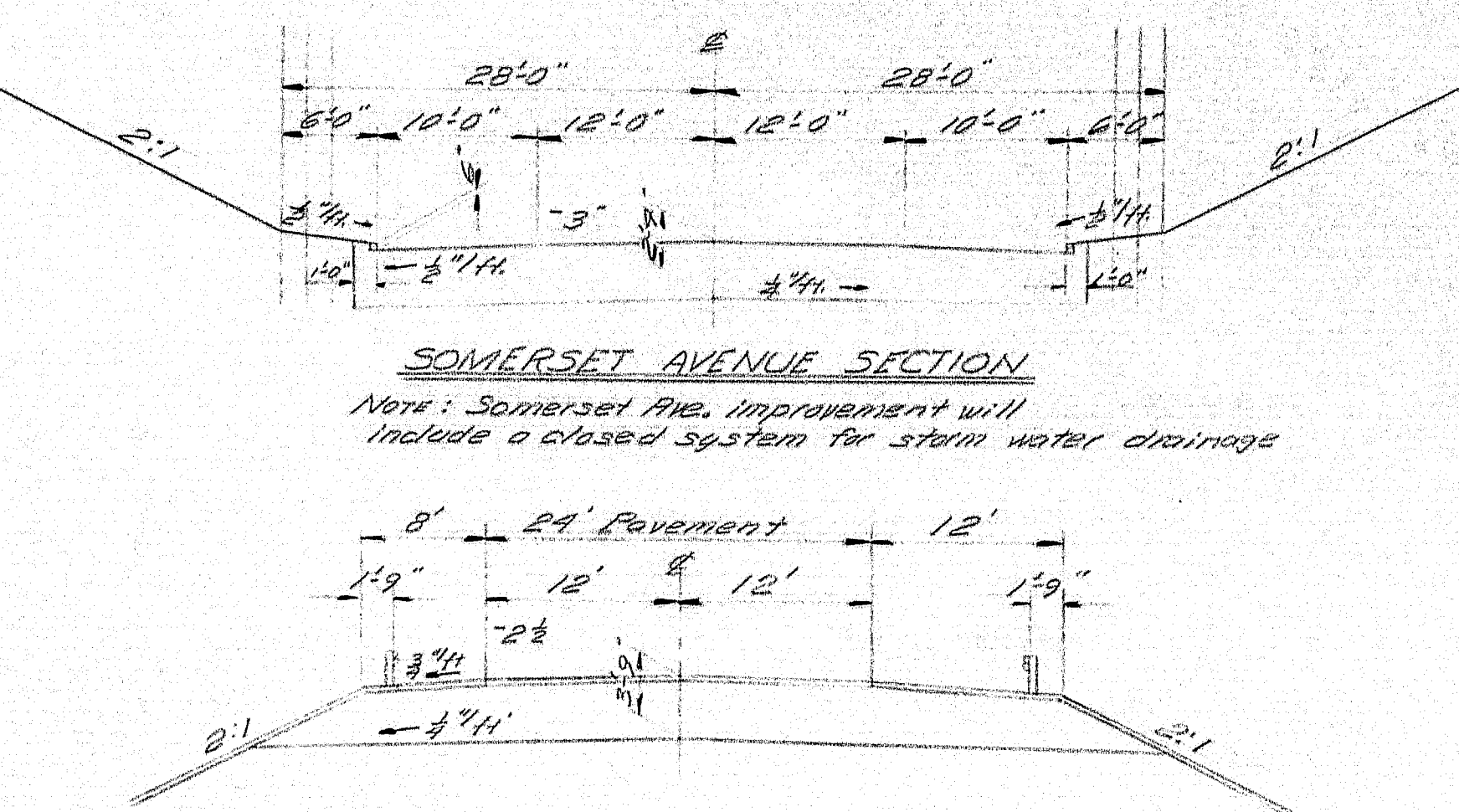
DESIGN - DETAIL - E.T.A.	BRIDGE NO. SURVEY - PLOT -
TRACE - H.L.D.	
STATE HIGHWAY COMMISSION BRIDGE DIVISION	
<b>INTERSTATE 95</b>	
OVER	
<b>SOMERSET AVENUE</b>	
IN THE TOWN OF	
<b>PITTSFIELD</b>	
<b>SOMERSET COUNTY</b>	
SLOPE PAVING - ALTERNATE	
SHEET 14 OF 16 AUGUSTA, MAINE AUGUST 1963	





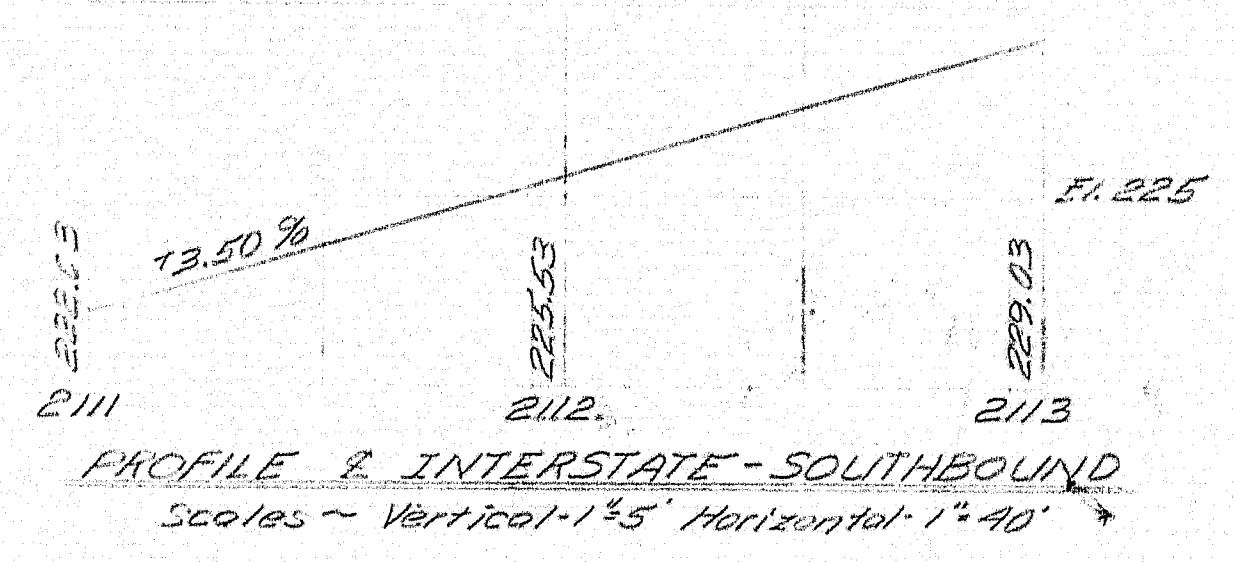


PLAN  
Scale = 1" = 20'

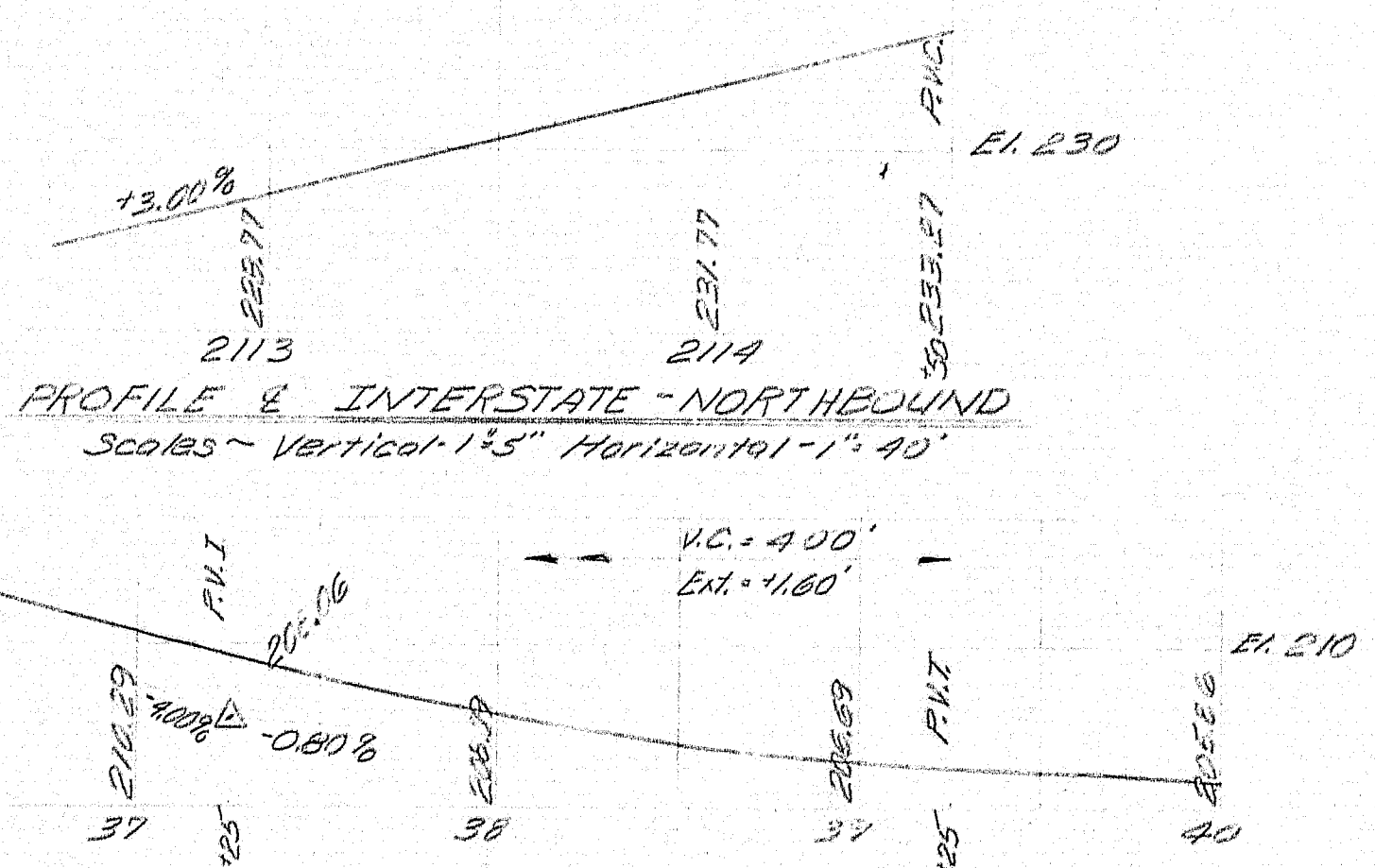


**SOMERSET AVENUE SECTION**  
Note: Somerset Ave. improvement will include a closed system for storm water drainage

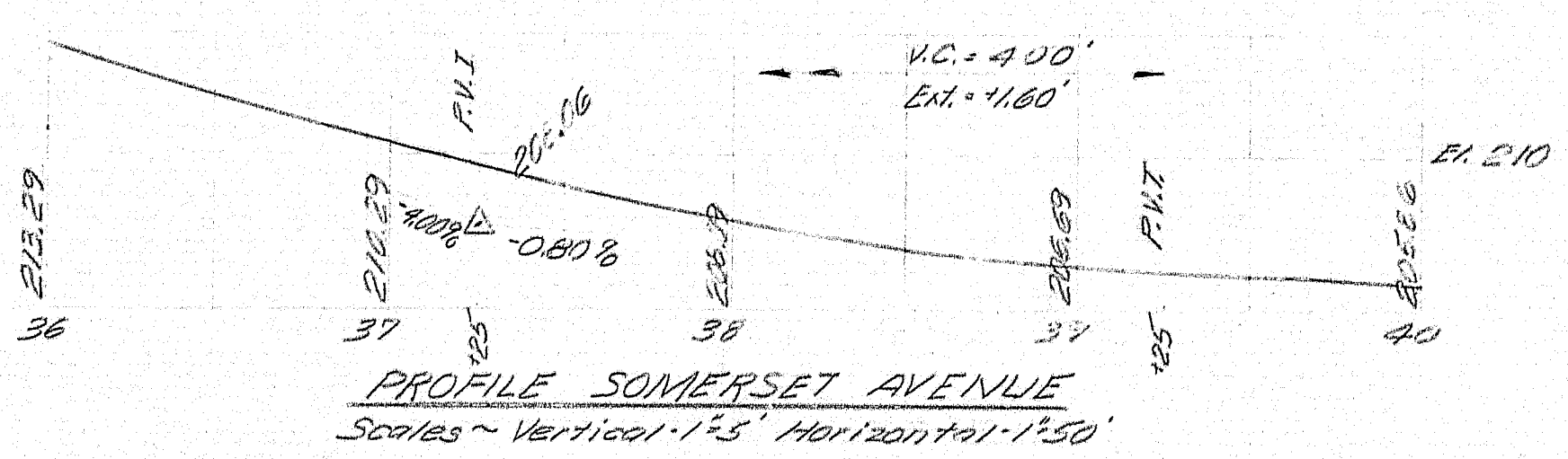
**TYPICAL INTERSTATE SECTION**  
WITH Traffic



**PROFILE & INTERSTATE - SOUTHBOUND**  
Scales ~ Vertical 1" = 5' Horizontal 1" = 40'



**PROFILE & INTERSTATE - NORTHBOUND**  
Scales ~ Vertical 1" = 5' Horizontal 1" = 40'

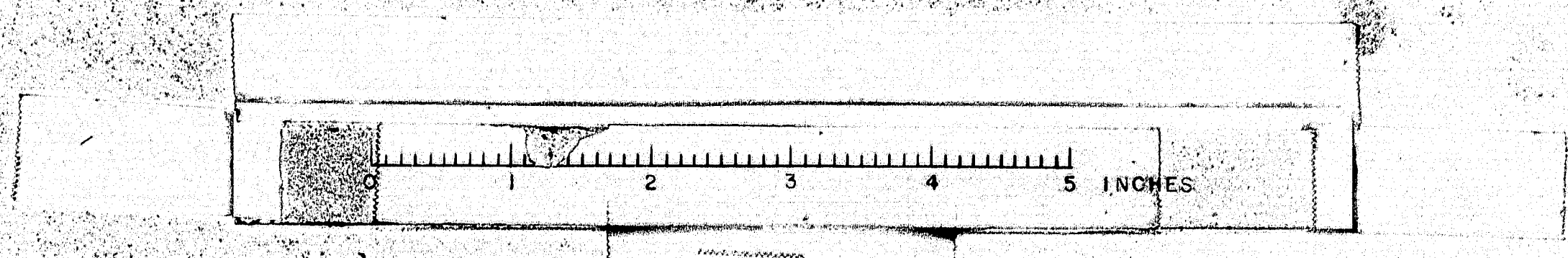


**PROFILE SOMERSET AVENUE**  
Scales ~ Vertical 1" = 5' Horizontal 1" = 50'

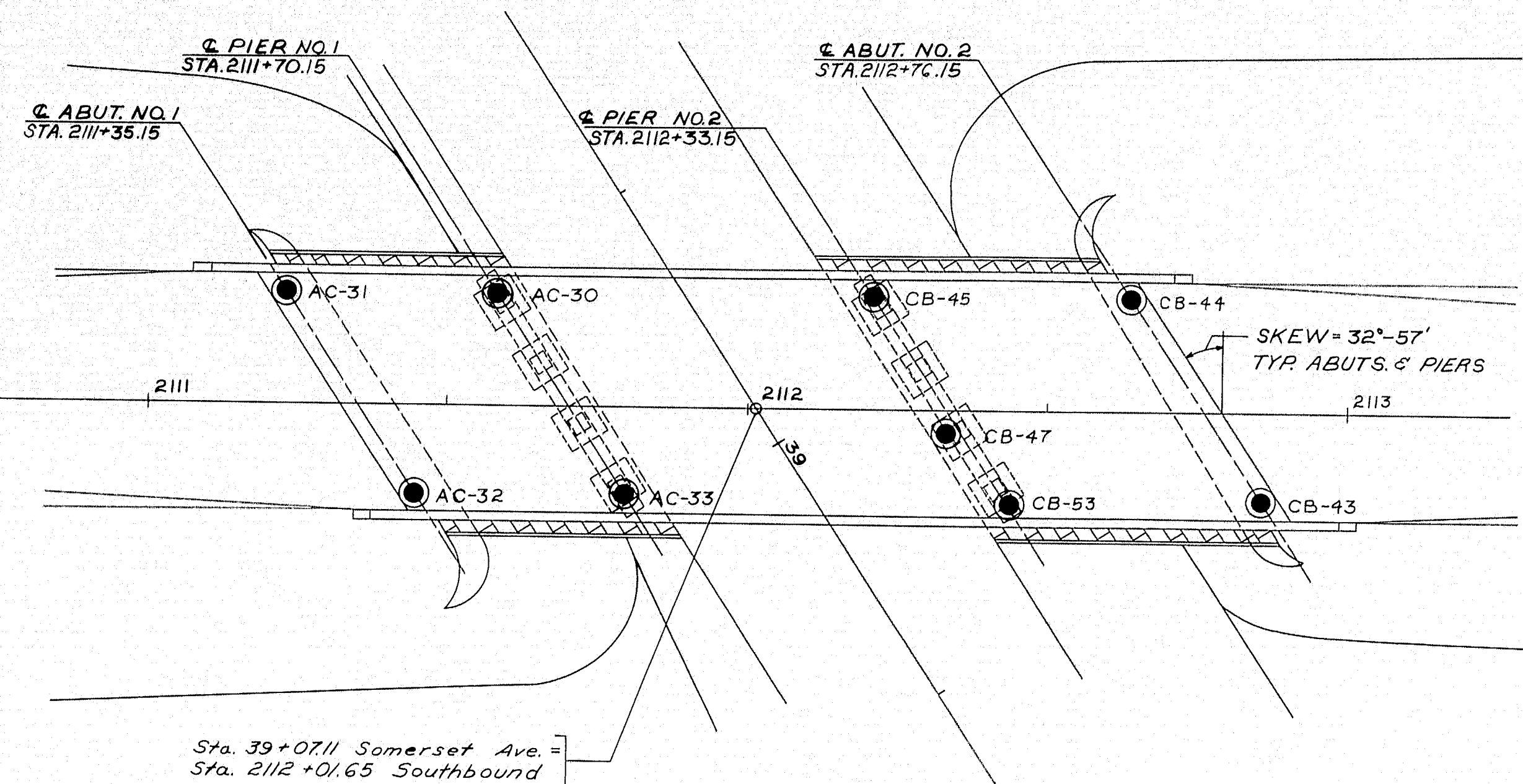
NOTE:  
Plan shows ultimate construction.  
Approach pavement not part  
of this contract

DESIGN - CDH TRACE - JPM CHECK - B.A.S.	
STATE HIGHWAY COMMISSION BRIDGE DIVISION	
INTERSTATE 95 OVER	
SOMERSET AVENUE IN THE TOWN OF PITTSFIELD SOMERSET COUNTY	
GENERAL PLAN	SHEET 30 OF 36
AUGUSTA, MAINE FEB. 1963	

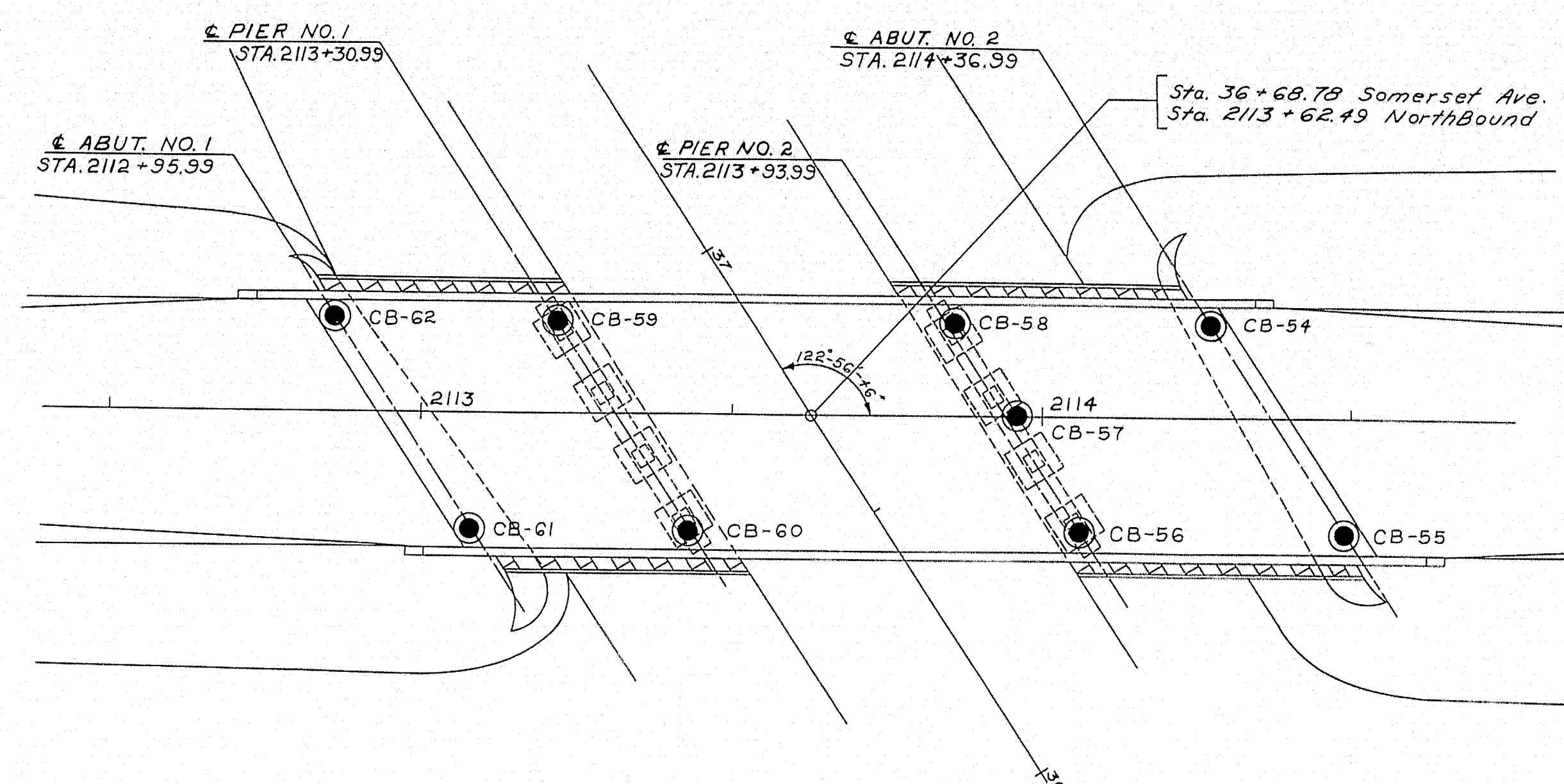
ELEVATION  
Vertical 1" = 20'  
Horizontal 1" = 20'



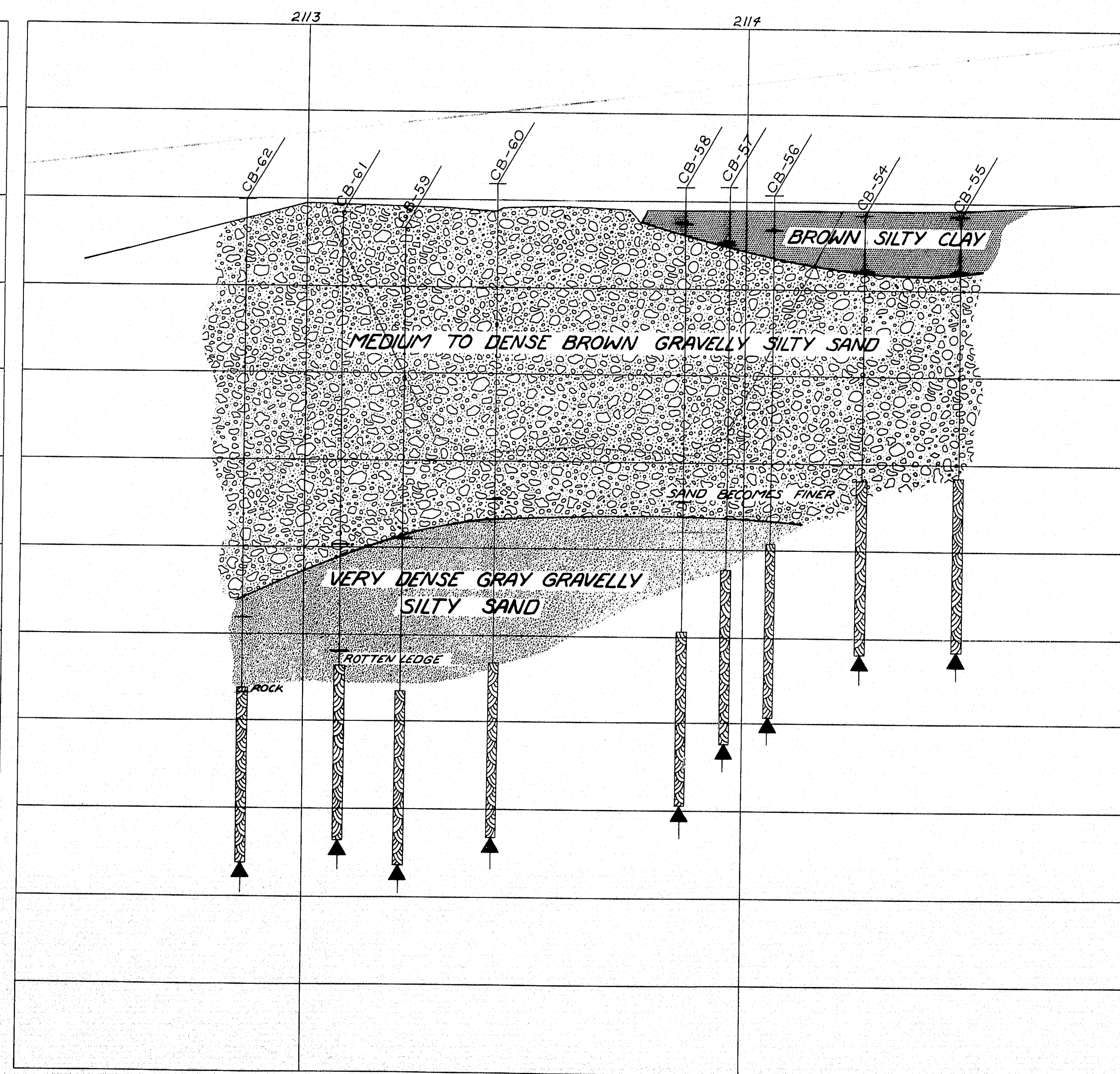
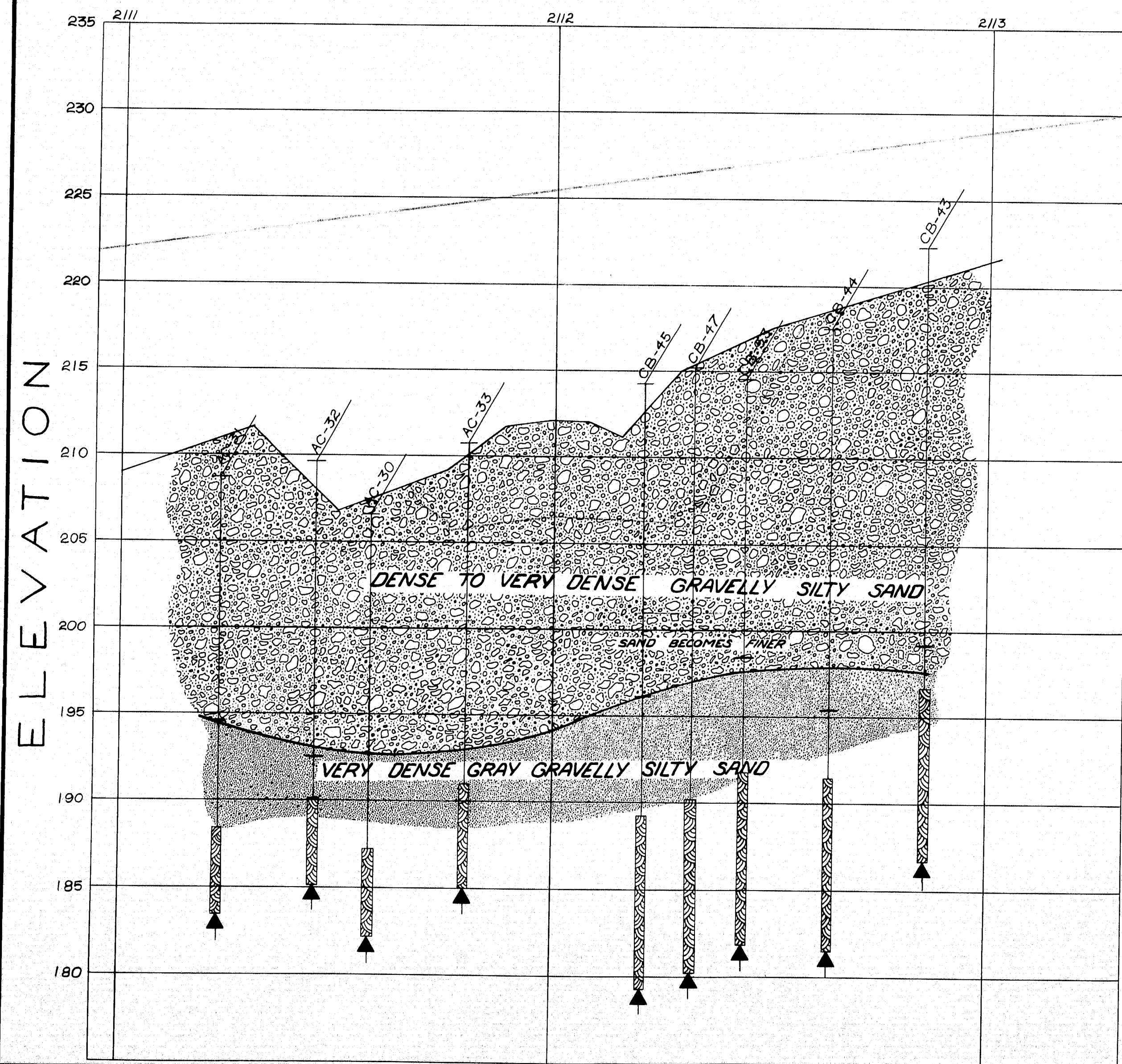




SOUTHBOUND LANE



NORTHBOUND LANE



**BORING NOTES**

ALL SAMPLES AND VANES ARE MADE AHEAD OF CASING.

NUMBER OF BLOWS REQUIRED TO DRIVE EXTRA HEAVY CASING ONE FOOT WITH 400 FT. LBS. OF ENERGY PER BLOW.

LOCATION OF SAMPLE OR SAMPLE ATTEMPT.

NUMBER AND TYPE OF DRY SAMPLE.

S & H SAMPLER #1290'S

UNSUCCESSFUL SAMPLE ATTEMPT AND TYPE OF SAMPLER

NUMBER OF BLOWS REQUIRED TO DRIVE SPOON OR TUBING ONE FOOT WITH 350 FT. LBS. OF ENERGY PER BLOW.

BOTTOM OF BORING (MAY NOT BE BOTTOM OF SOIL STRATA)

LOCATIONS CORED BY DIAMOND BIT AND PER CENT RECOVERY OF ROCK.

DESIGN—  
TRACE—  
CHECK—

SOILS DIVISION

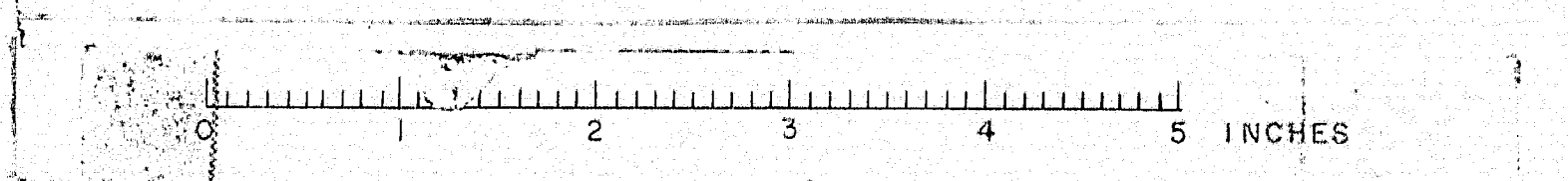
BRIDGE NO.  
SURVEY—  
PLOT—

STATE HIGHWAY COMMISSION  
BRIDGE DIVISION

**INTERSTATE 95  
OVER  
SOMERSET AVENUE  
IN THE TOWN OF  
PITTSFIELD  
SOMERSET COUNTY  
FOUNDATION SURVEY**

SHEET 3 OF 16 AUGUSTA, MAINE JAN. 1963

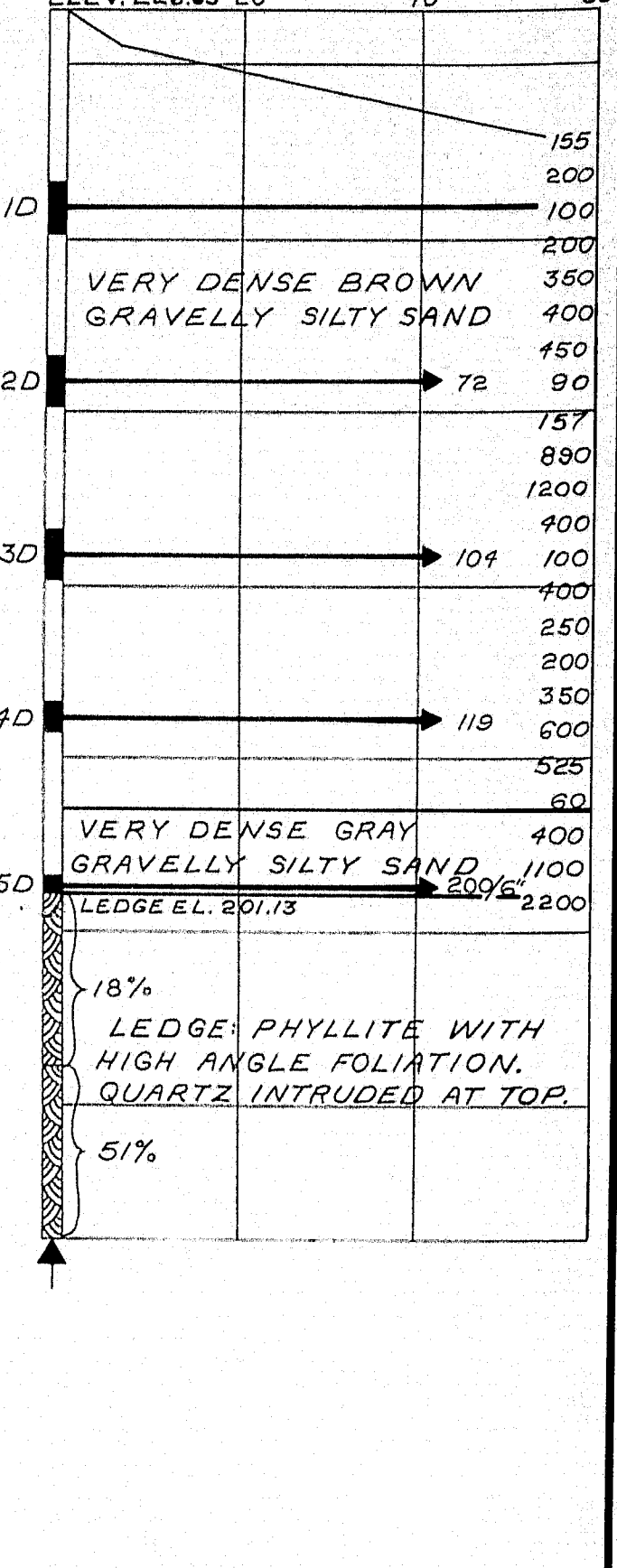
M-1966





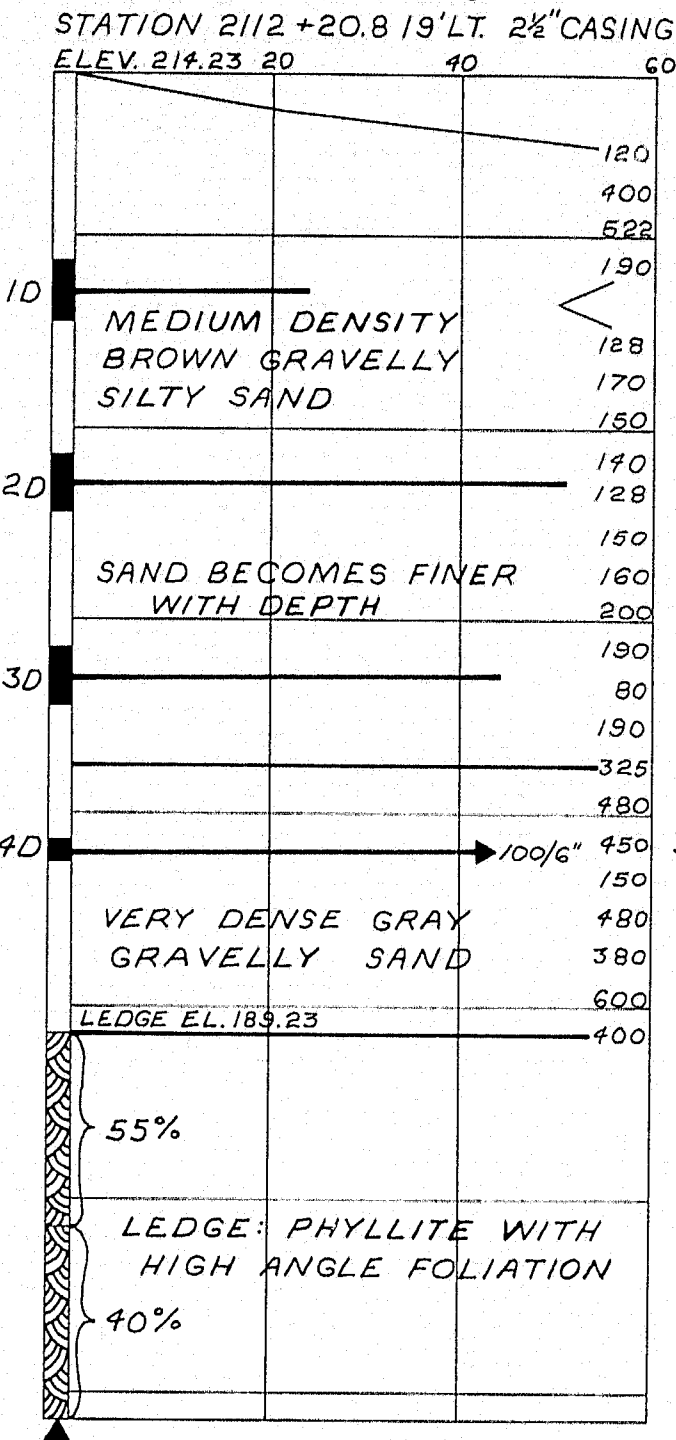
# ABUTMENT NO. 2 S.B.L.

BORING CB-43  
STATION 2112+85.9 15' RT. 2 1/2" CASING  
ELEV. 226.63 20 40 60

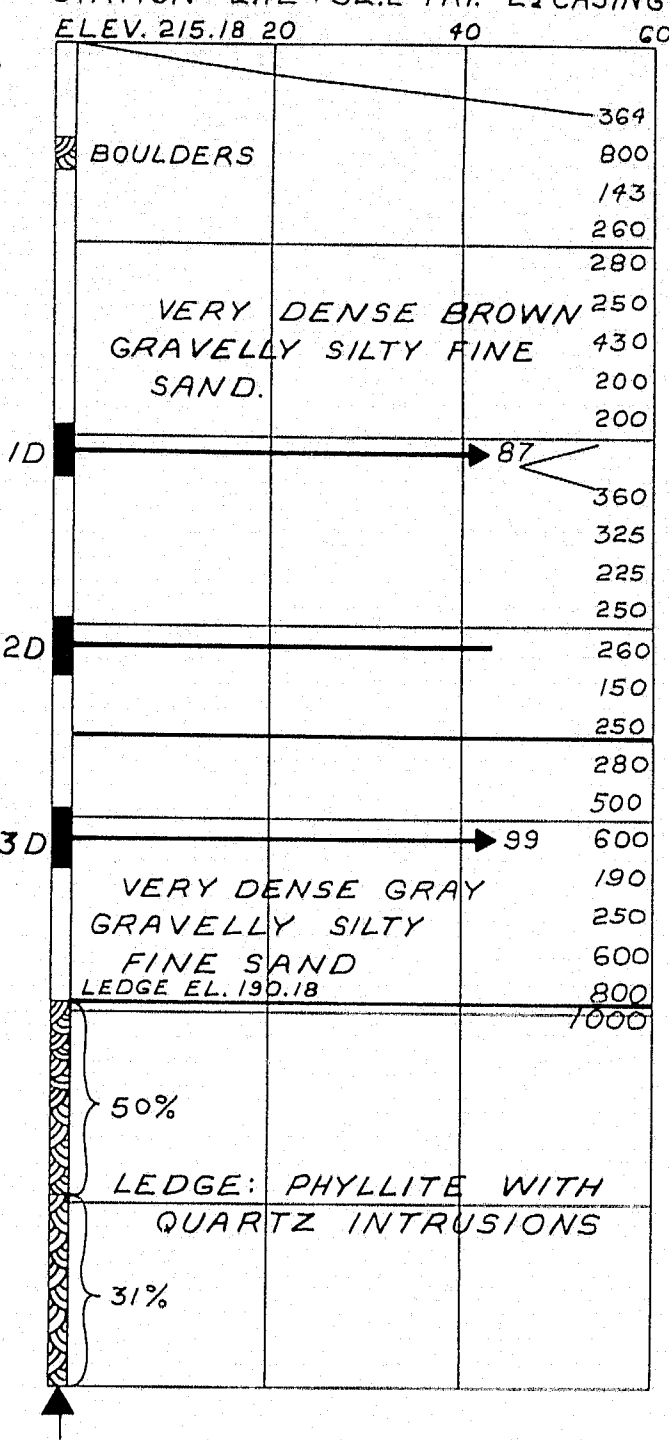


# PIER NO. 2 S.B.L.

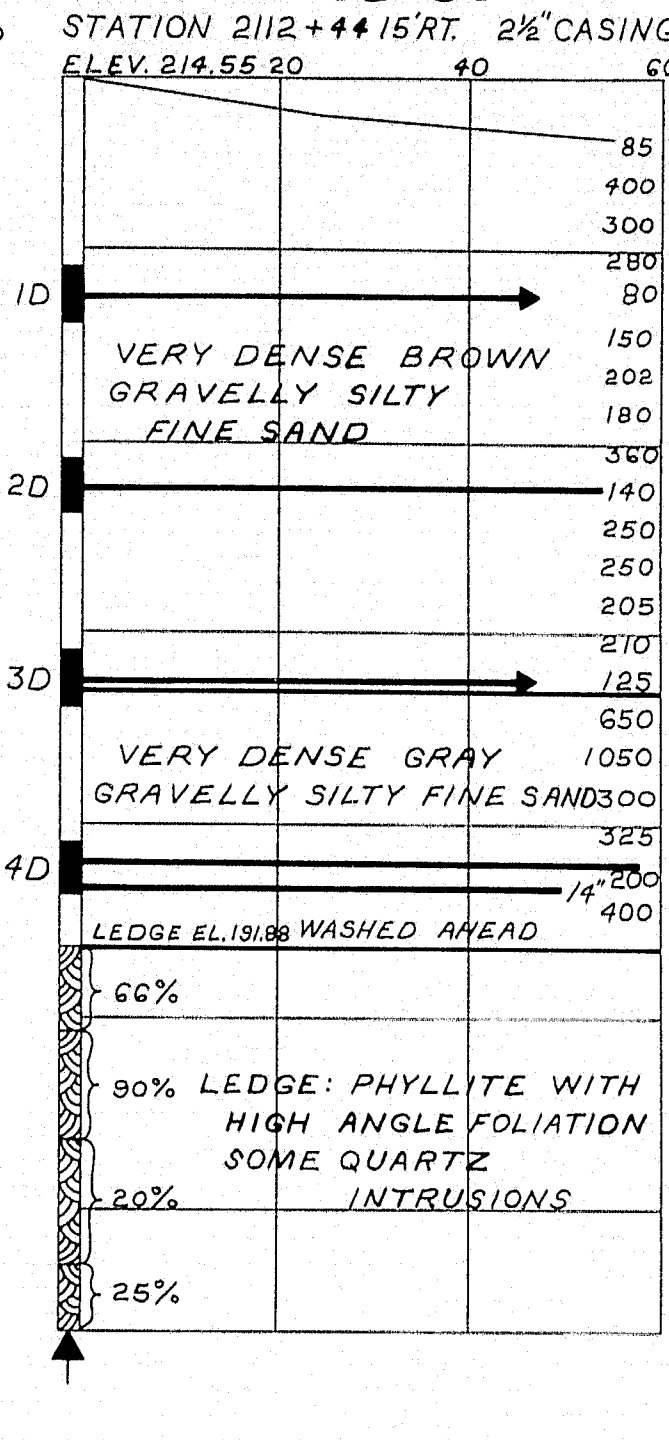
BORING CB-45  
STATION 2112+20.8 19' LT. 2 1/2" CASING  
ELEV. 214.23 20 40 60



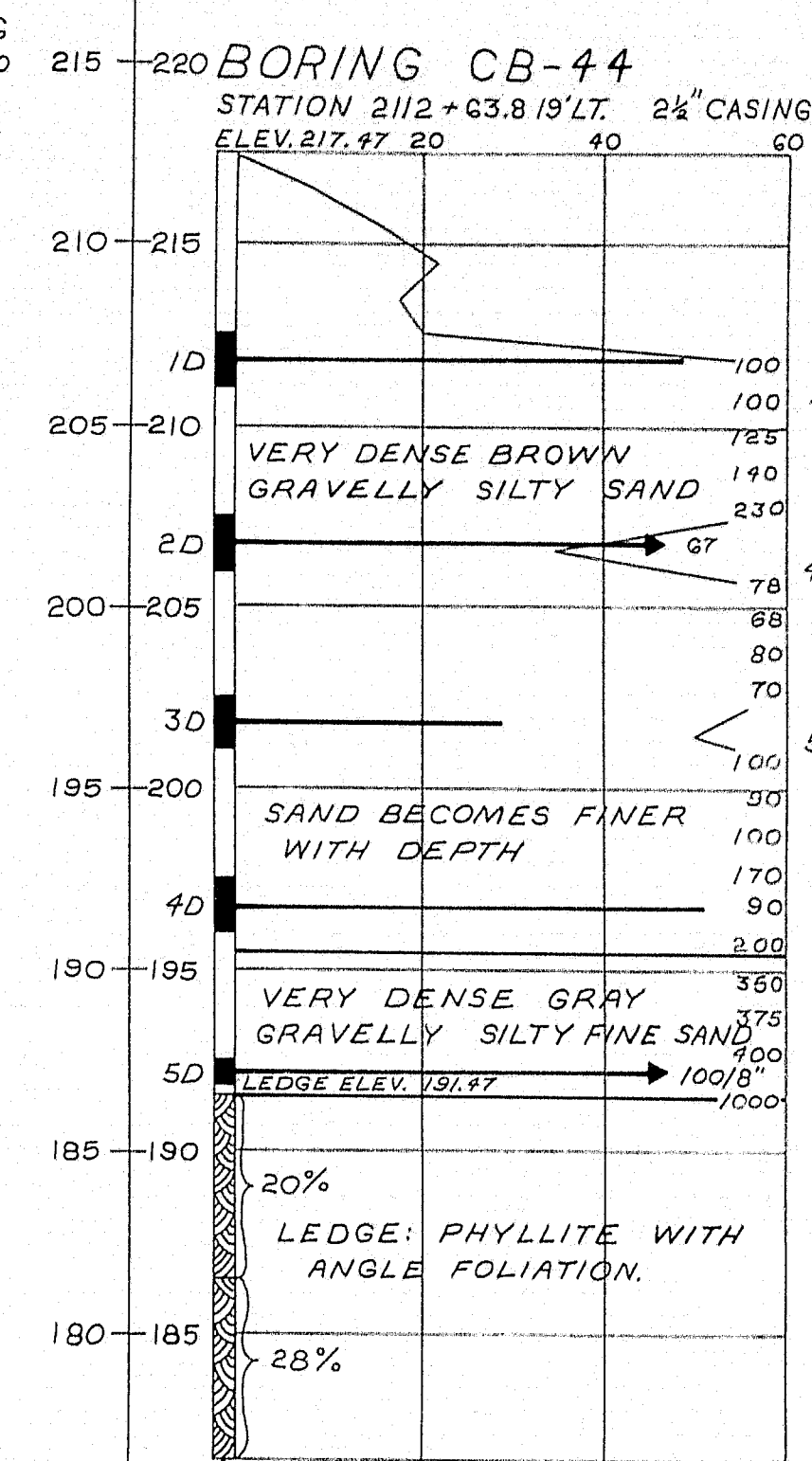
BORING CB-47  
STATION 2112+32.2 17' RT. 2 1/2" CASING  
ELEV. 215.18 20 40 60



BORING CB-53  
STATION 2112+44.15' RT. 2 1/2" CASING  
ELEV. 214.55 20 40 60

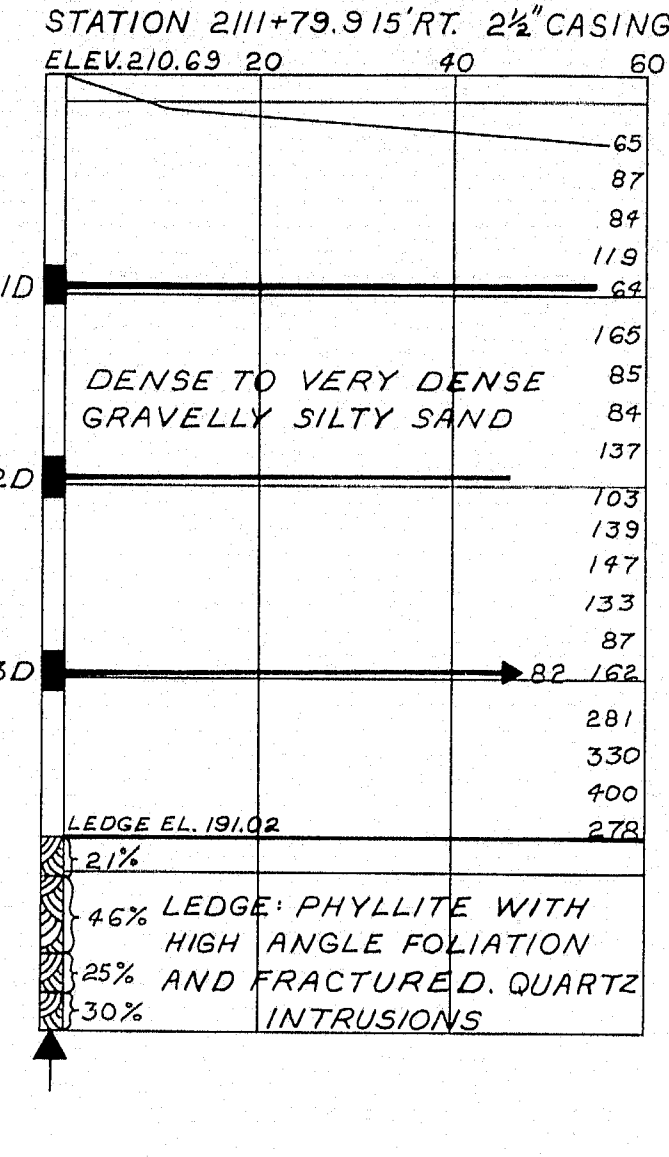


BORING CB-44  
STATION 2112+63.8 19' LT. 2 1/2" CASING  
ELEV. 217.97 20 40 60

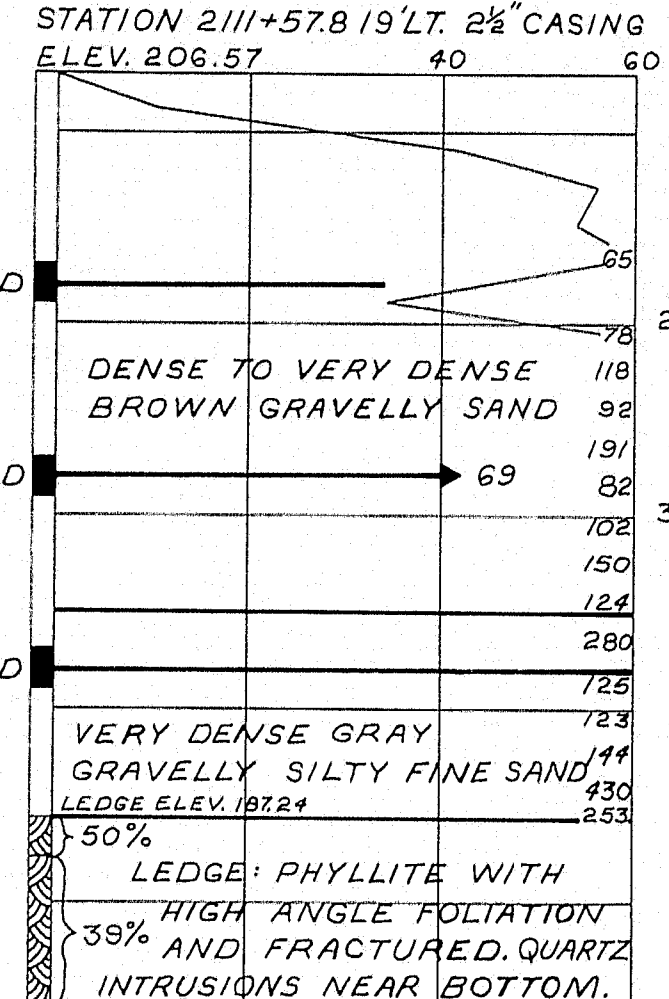


# PIER NO. 1 S.B.L.

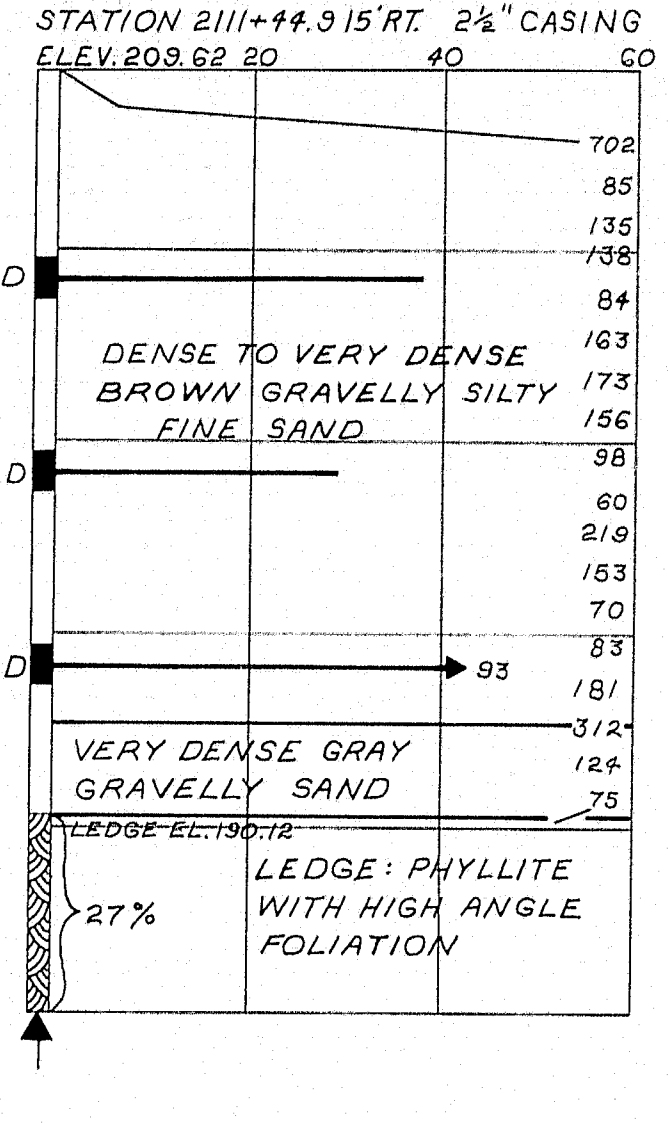
BORING AC-33  
STATION 2111+79.9 15' RT. 2 1/2" CASING  
ELEV. 210.69 20 40 60



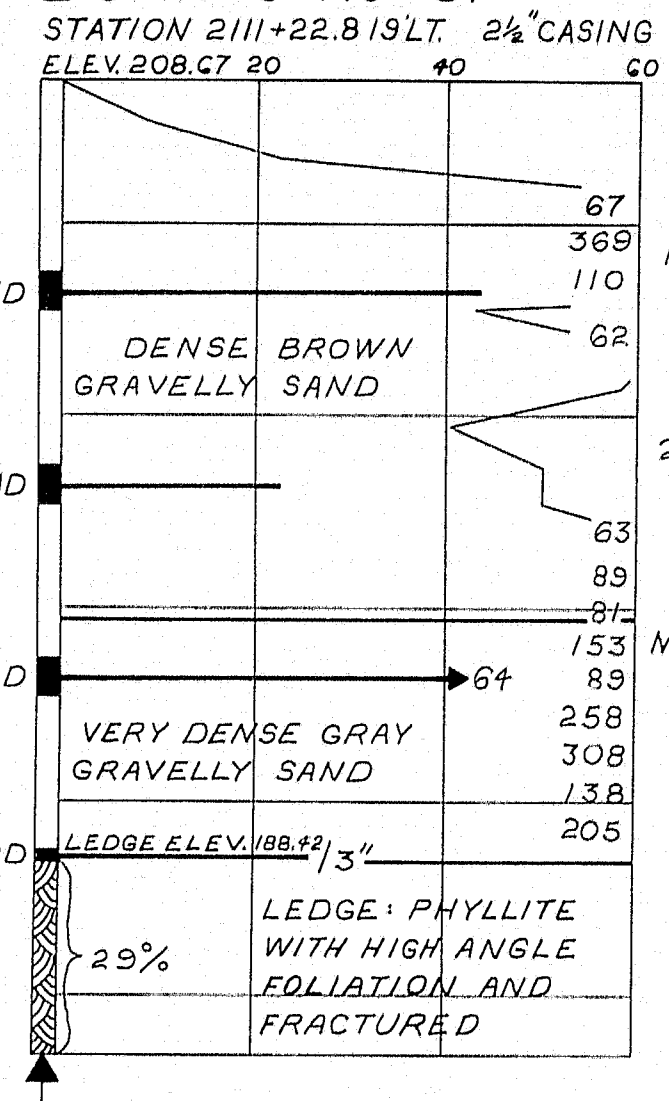
BORING AC-30  
STATION 2111+57.8 19' LT. 2 1/2" CASING  
ELEV. 206.57 20 40 60



BORING AC-32  
STATION 2111+44.9 15' RT. 2 1/2" CASING  
ELEV. 209.62 20 40 60



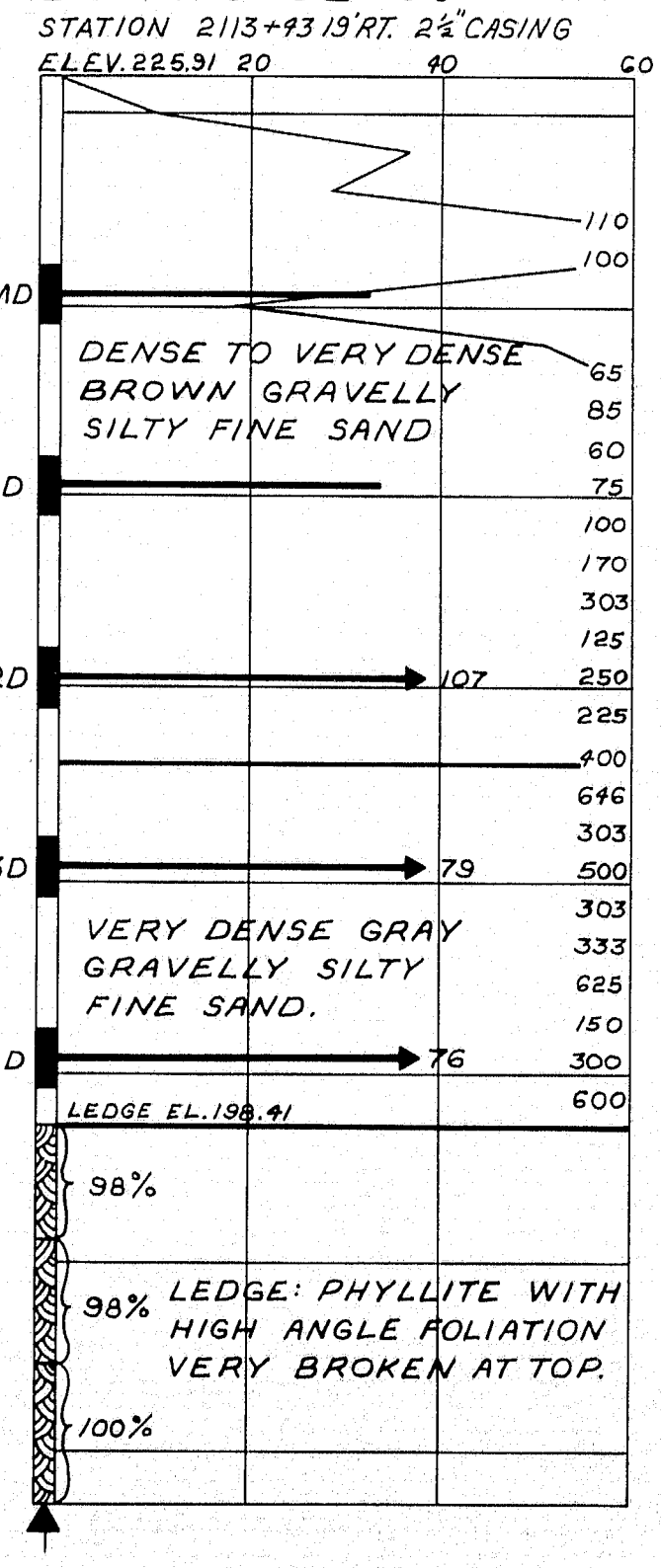
BORING AC-31  
STATION 2111+22.8 19' LT. 2 1/2" CASING  
ELEV. 208.67 20 40 60



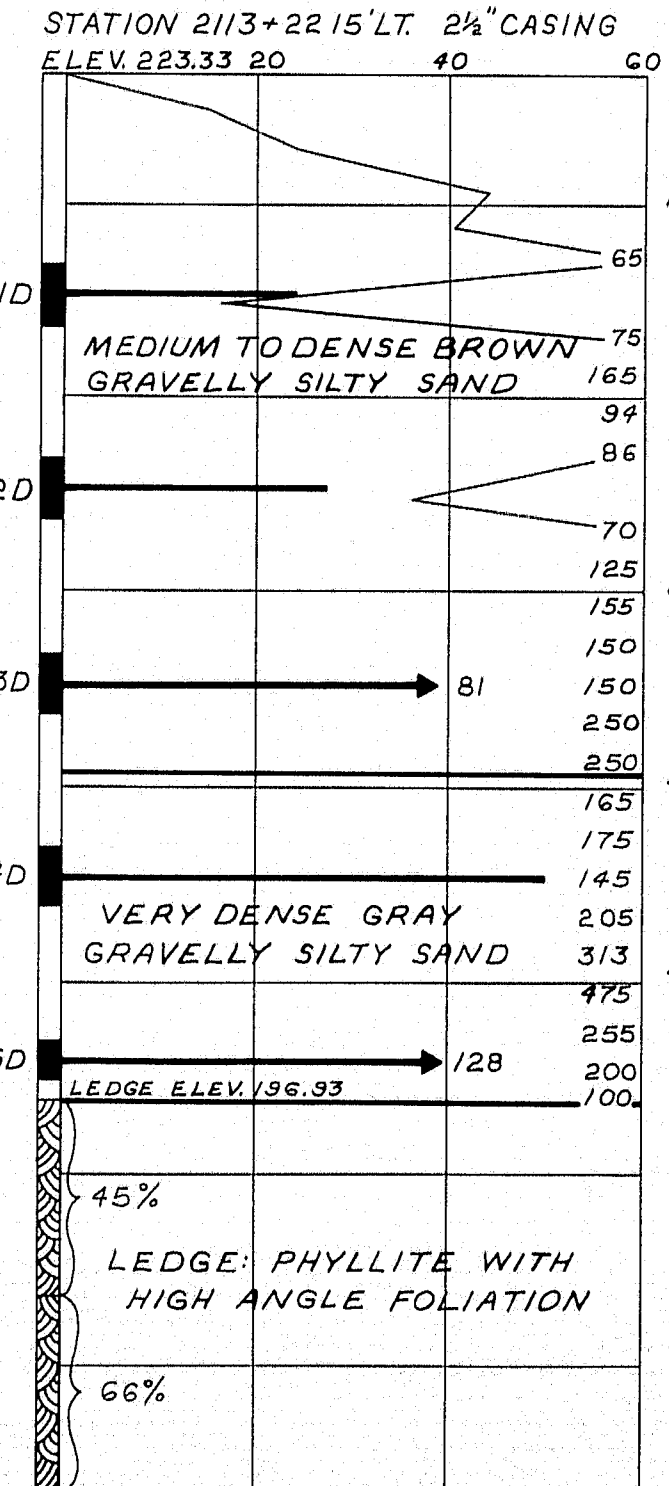
# ABUTMENT NO. 1 S.B.L.

# PIER NO. 1 N.B.L.

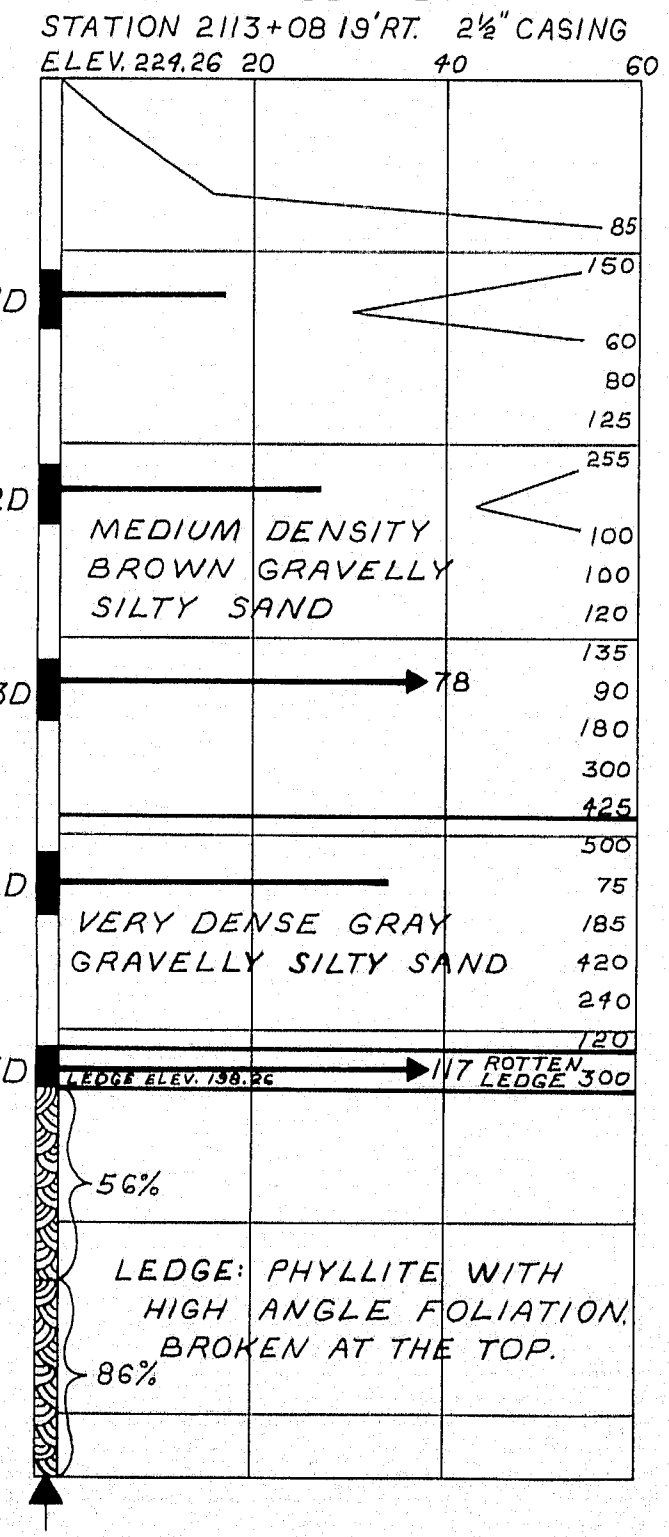
BORING CB-60  
STATION 2113+93 19' RT. 2 1/2" CASING  
ELEV. 225.91 20 40 60



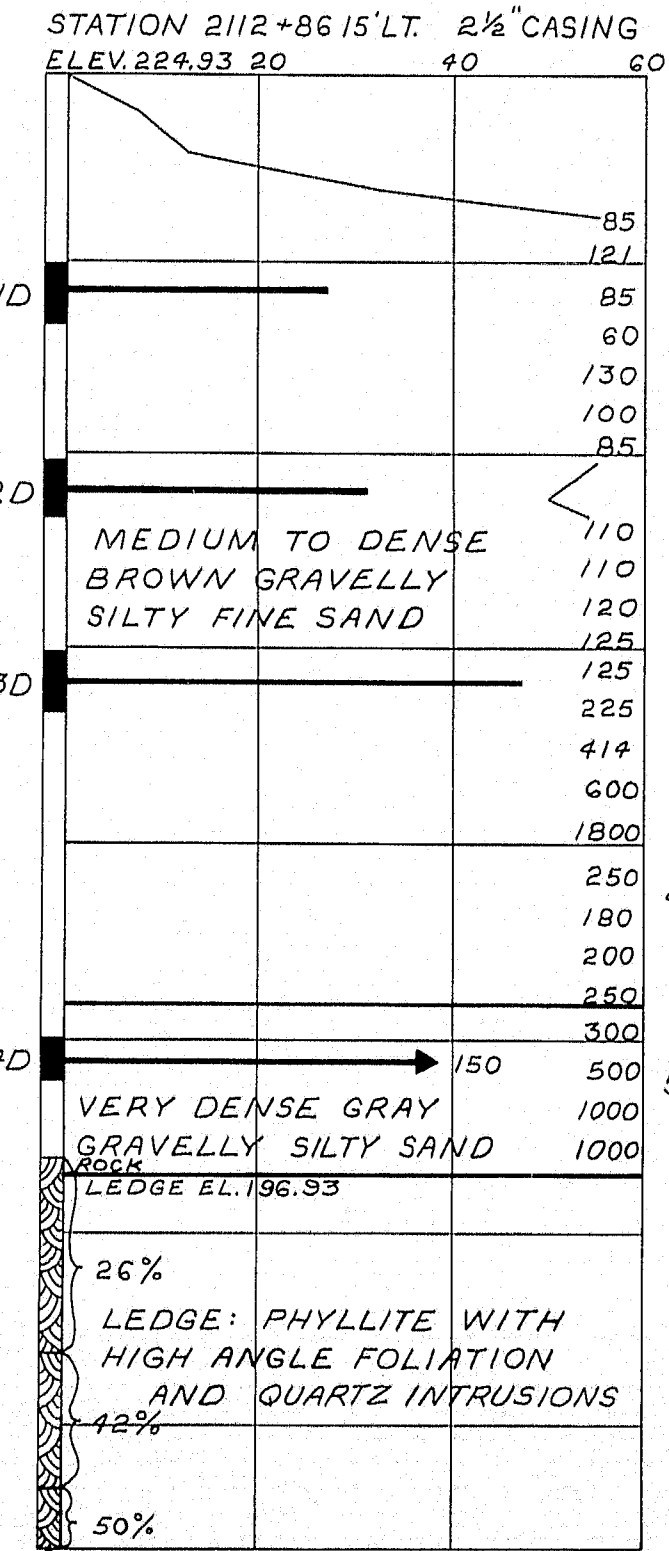
BORING CB-59  
STATION 2113+22 15' LT. 2 1/2" CASING  
ELEV. 223.33 20 40 60



BORING CB-61  
STATION 2113+08 19' RT. 2 1/2" CASING  
ELEV. 224.26 20 40 60



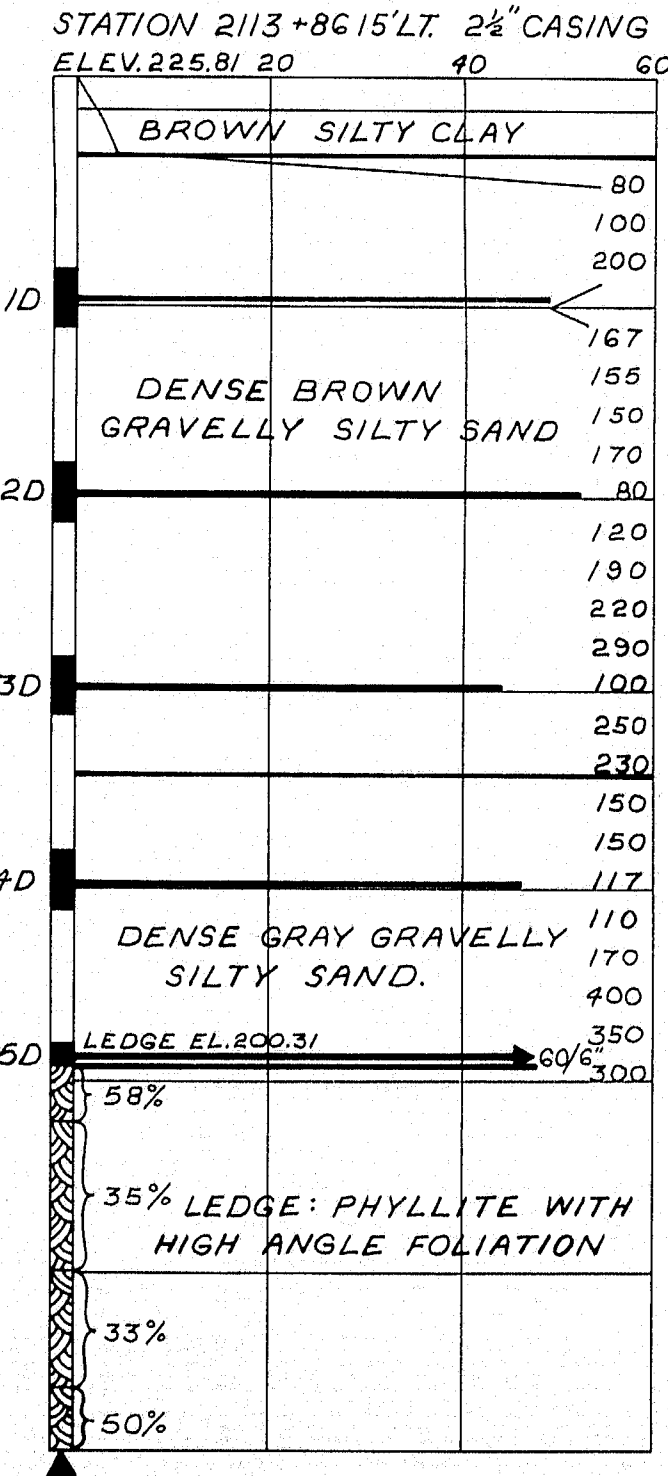
BORING CB-62  
STATION 2112+86 15' LT. 2 1/2" CASING  
ELEV. 224.93 20 40 60



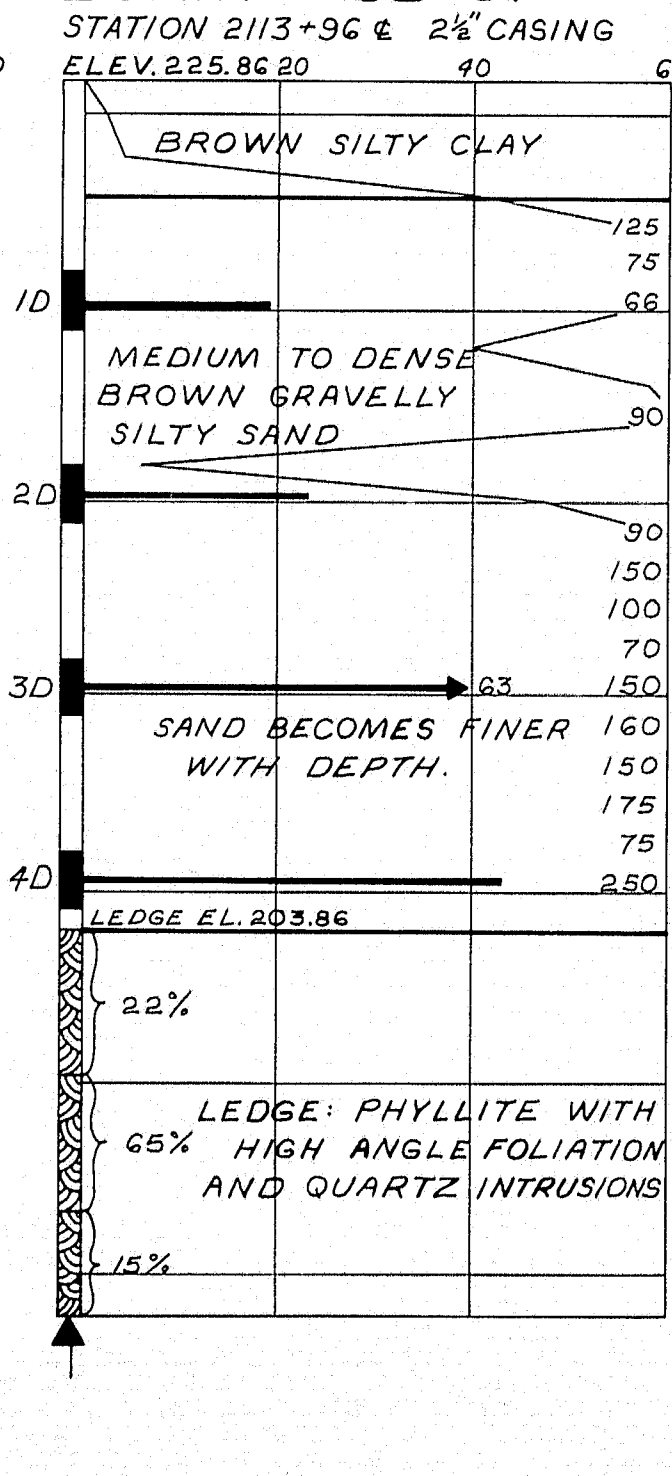
# ABUTMENT NO. 1 N.B.L.

# PIER NO. 2 N.B.L.

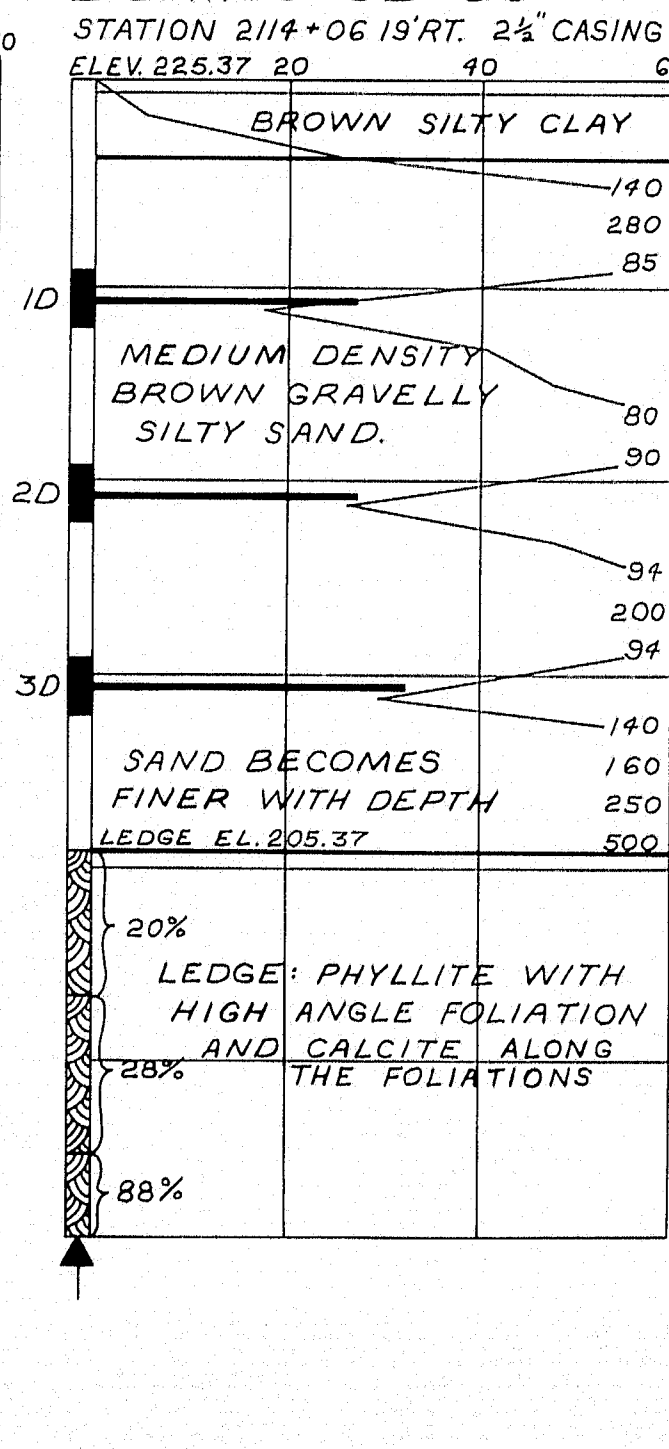
BORING CB-58  
STATION 2113+86 15' LT. 2 1/2" CASING  
ELEV. 225.81 20 40 60



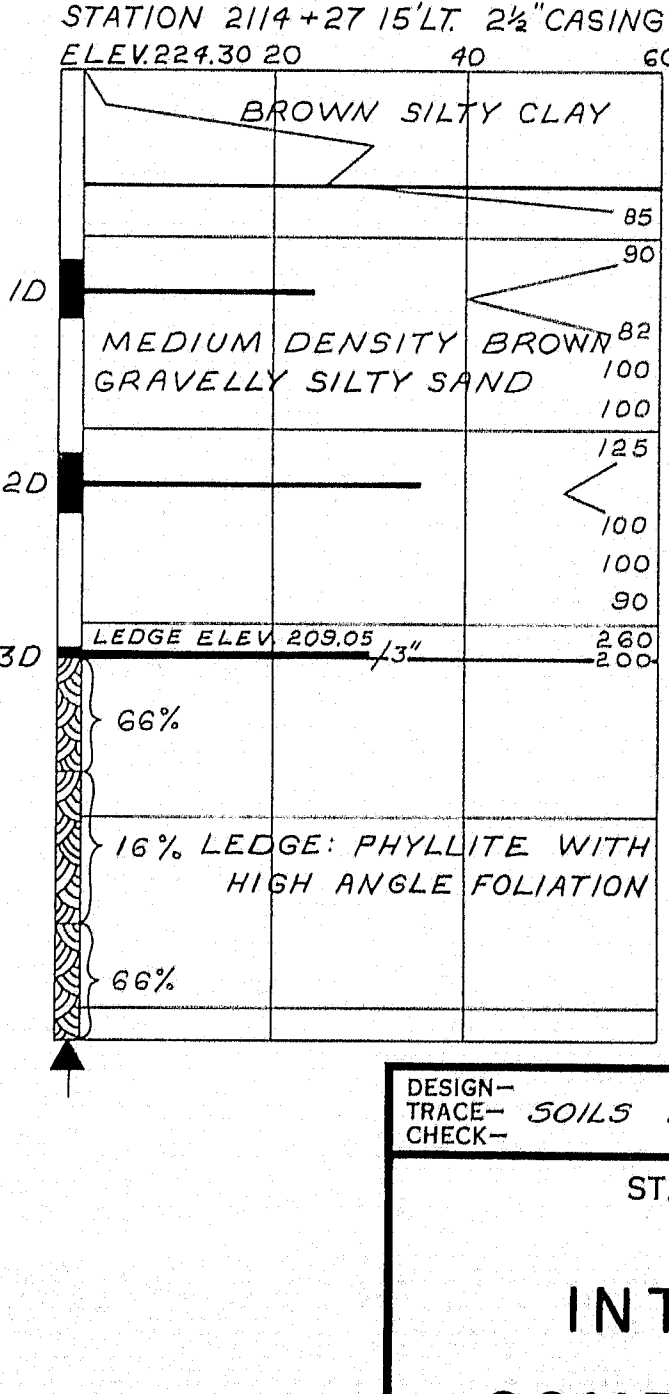
BORING CB-57  
STATION 2113+96 15' LT. 2 1/2" CASING  
ELEV. 225.86 20 40 60



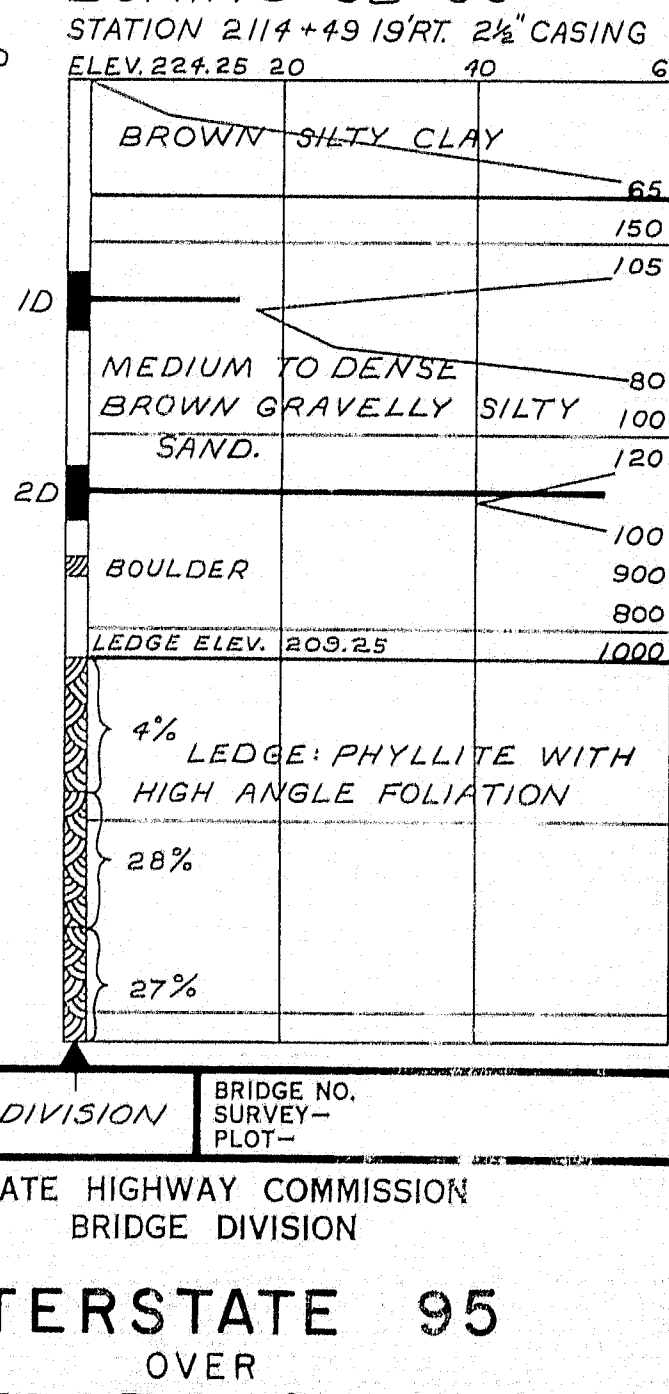
BORING CB-56  
STATION 2114+06 19' RT. 2 1/2" CASING  
ELEV. 225.37 20 40 60



BORING CB-54  
STATION 2114+27 15' LT. 2 1/2" CASING  
ELEV. 224.30 20 40 60



BORING CB-55  
STATION 2114+49 19' RT. 2 1/2" CASING  
ELEV. 224.25 20 40 60

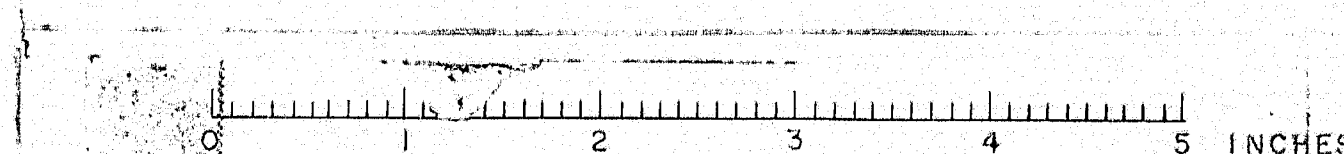


# ABUTMENT NO. 2 N.B.L.

STATE HIGHWAY COMMISSION  
BRIDGE DIVISION  
**INTERSTATE 95**  
OVER  
**SOMERSET AVENUE**  
IN THE TOWN OF  
**PITTSFIELD**  
SOMERSET COUNTY  
BORING DETAILS

SHEET 4 OF 16 AUGUSTA, MAINE JAN. 1963

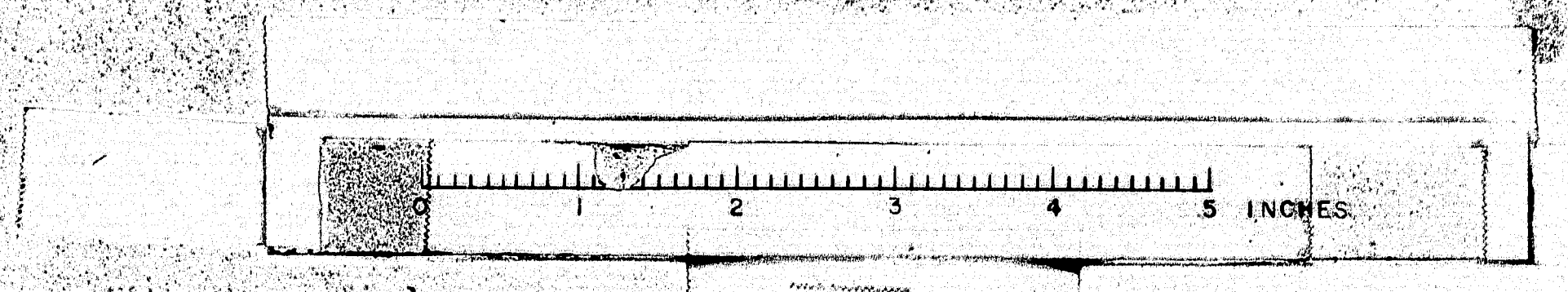
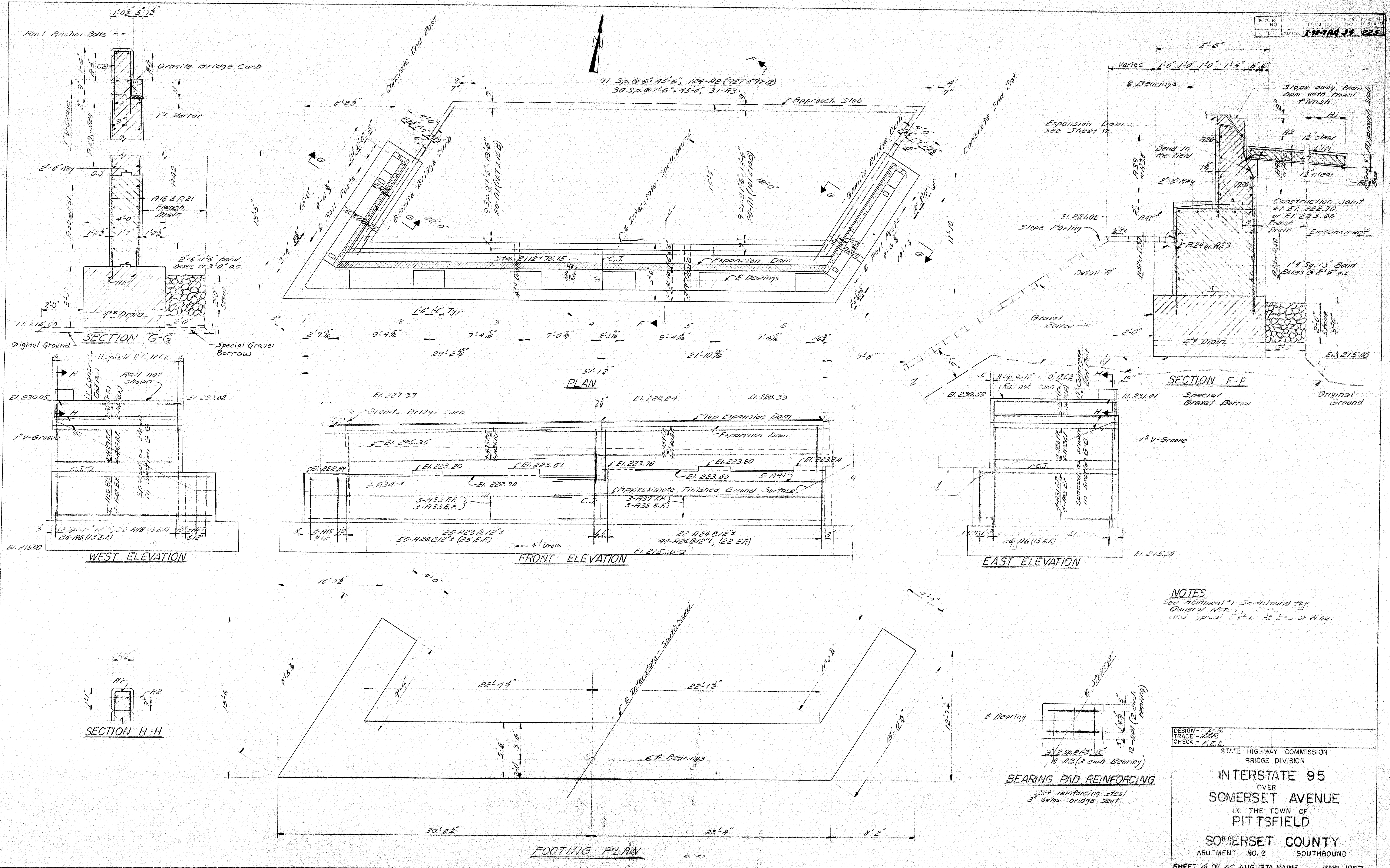
M-1967



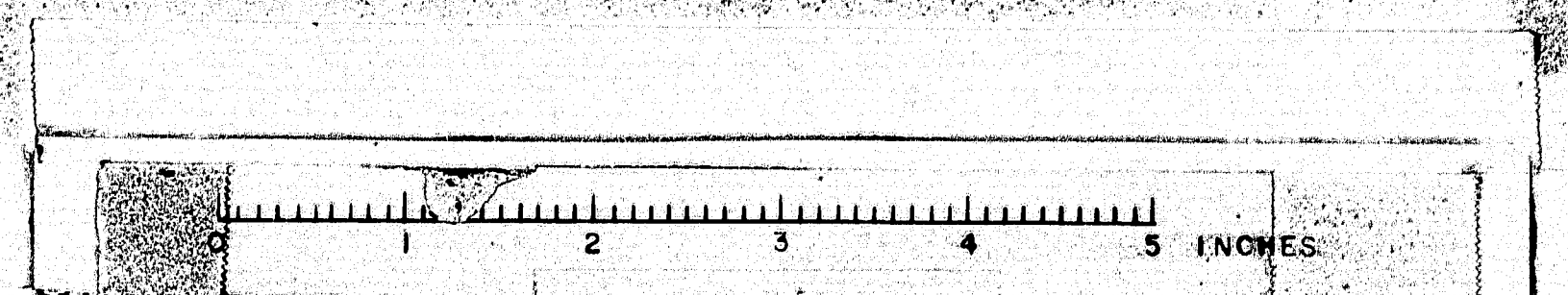
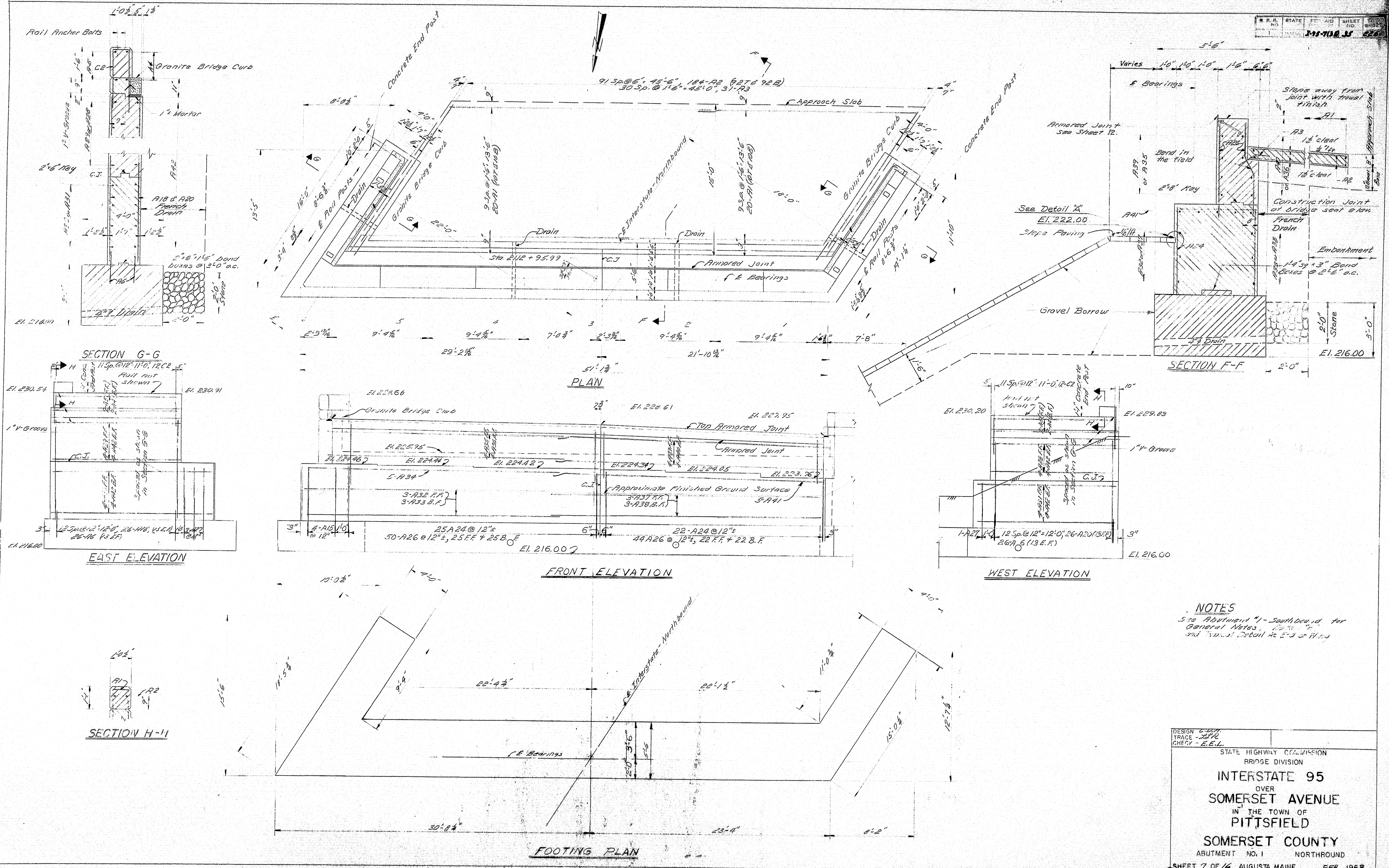




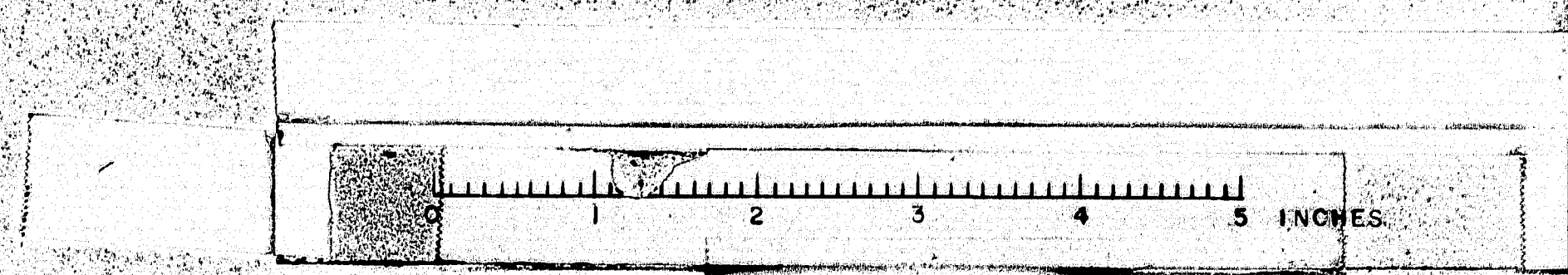
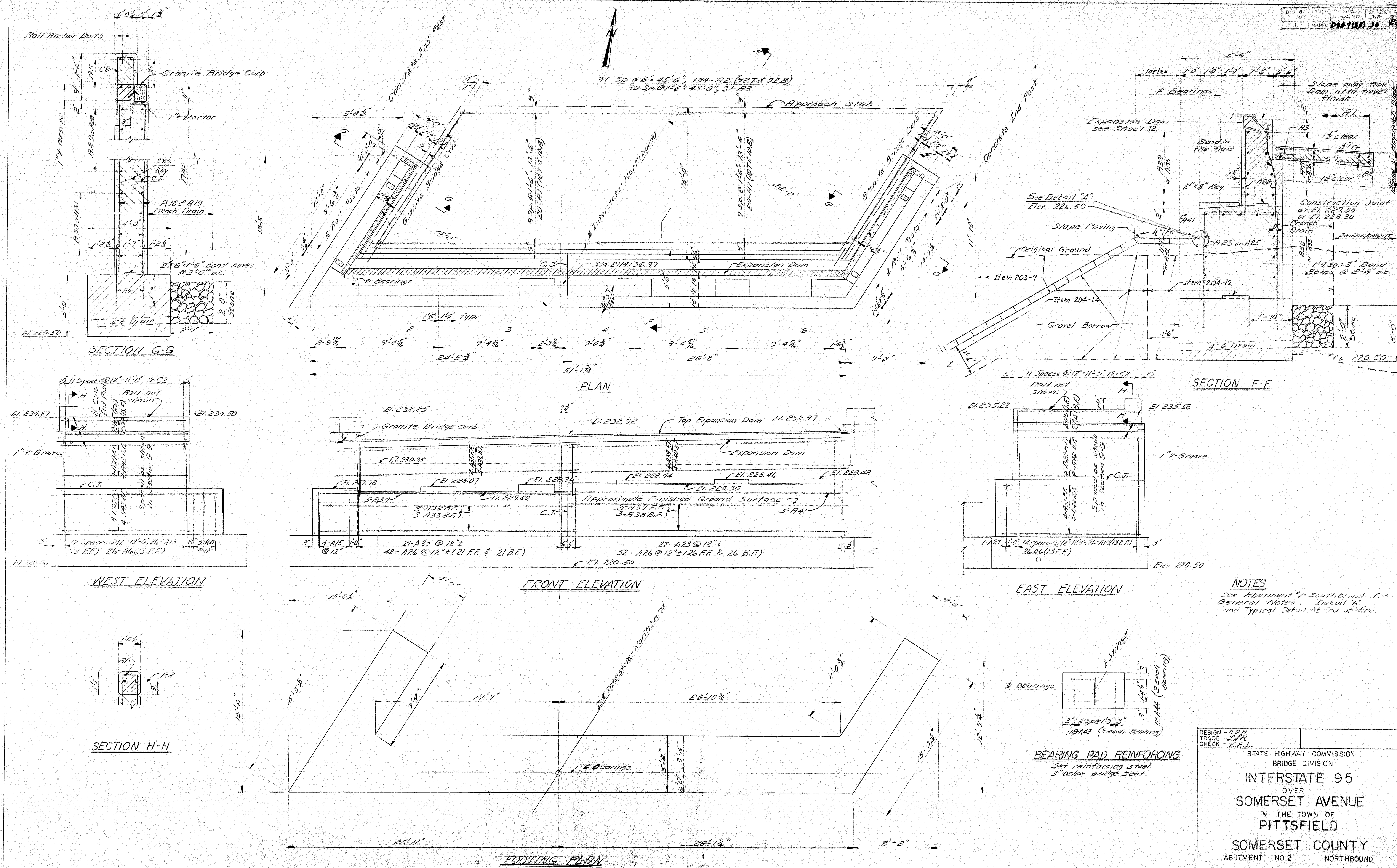




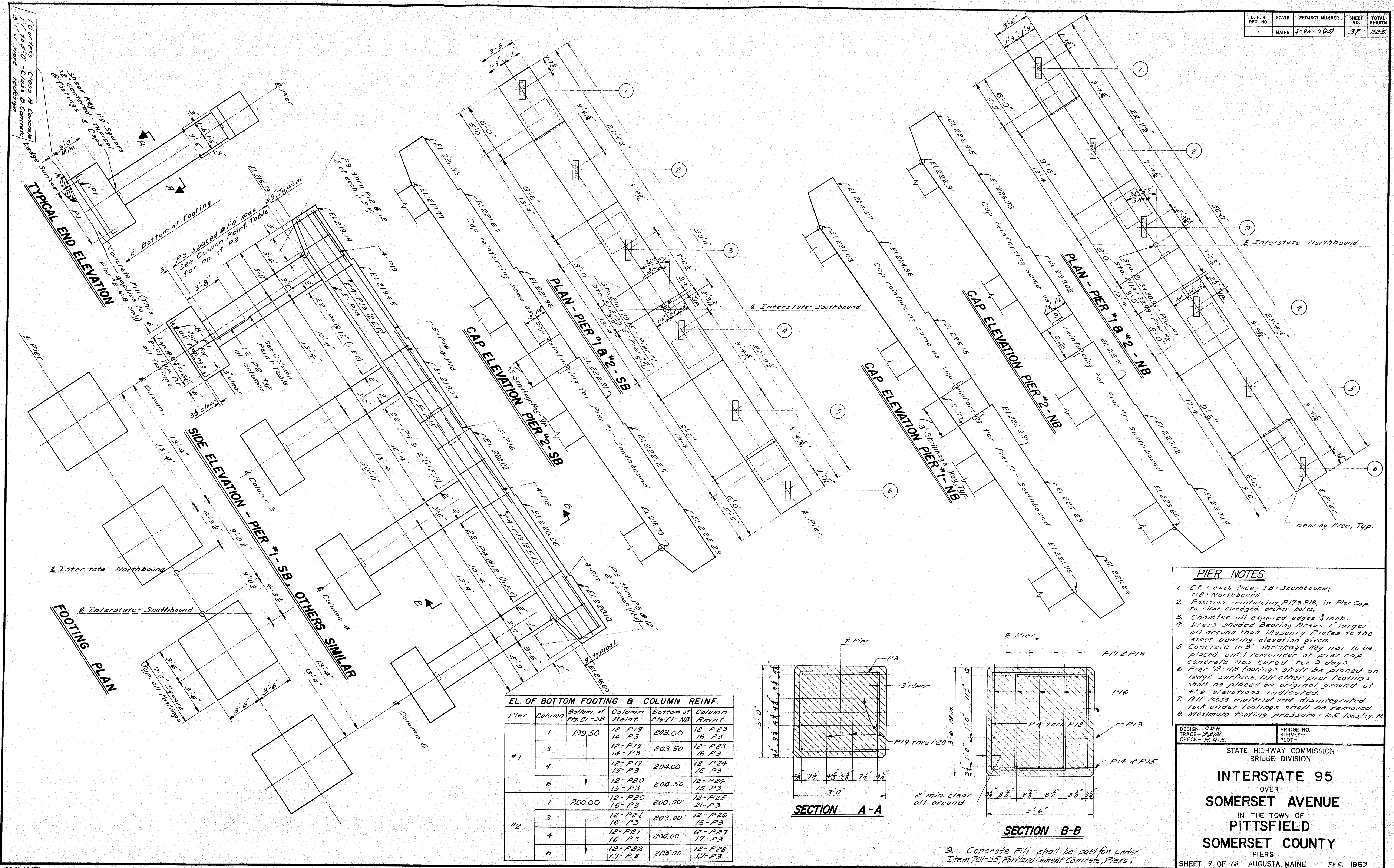












**PIER NOTES**

- E.F. = each face; SB = Southbound; NB = Northbound
- Position reinforcing, P17 & P18, in Pier Cap to clear swaged anchor bolts.
- Chamfer all exposed edges 3/4 inch.
- Dress Shaded Bearing Area 1' larger all around than Masonry Plates to the exact bearing elevation given.
- Concrete in 3' shrinkage key not to be placed until remainder of pier cap concrete has cured for 3 days.
- Pier #2-NB footings shall be placed on ledge surface. All other pier footings shall be placed on original ground at the elevations indicated.
- All loose material and disintegrated rock under footings shall be removed.
- Maximum footing pressure = 2.5 tons/sq. ft.

DESIGN - C.D.H.  
TRACE - S.H.P.  
CHECK - R.A.S.

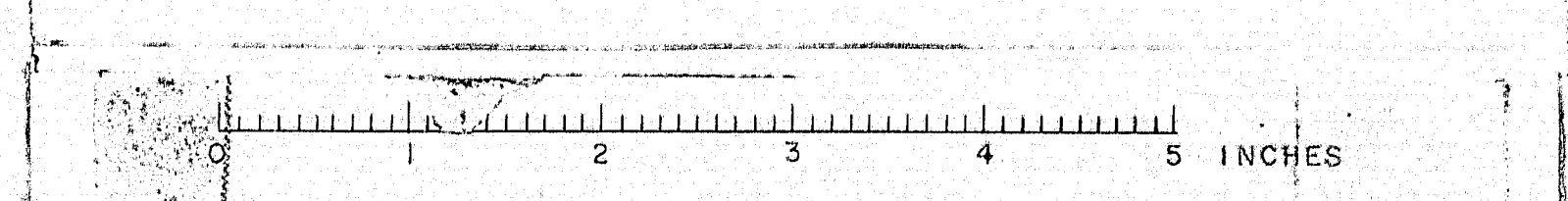
BRIDGE NO. 111  
SURVEY PLOT

STATE HIGHWAY COMMISSION  
BRIDGE DIVISION

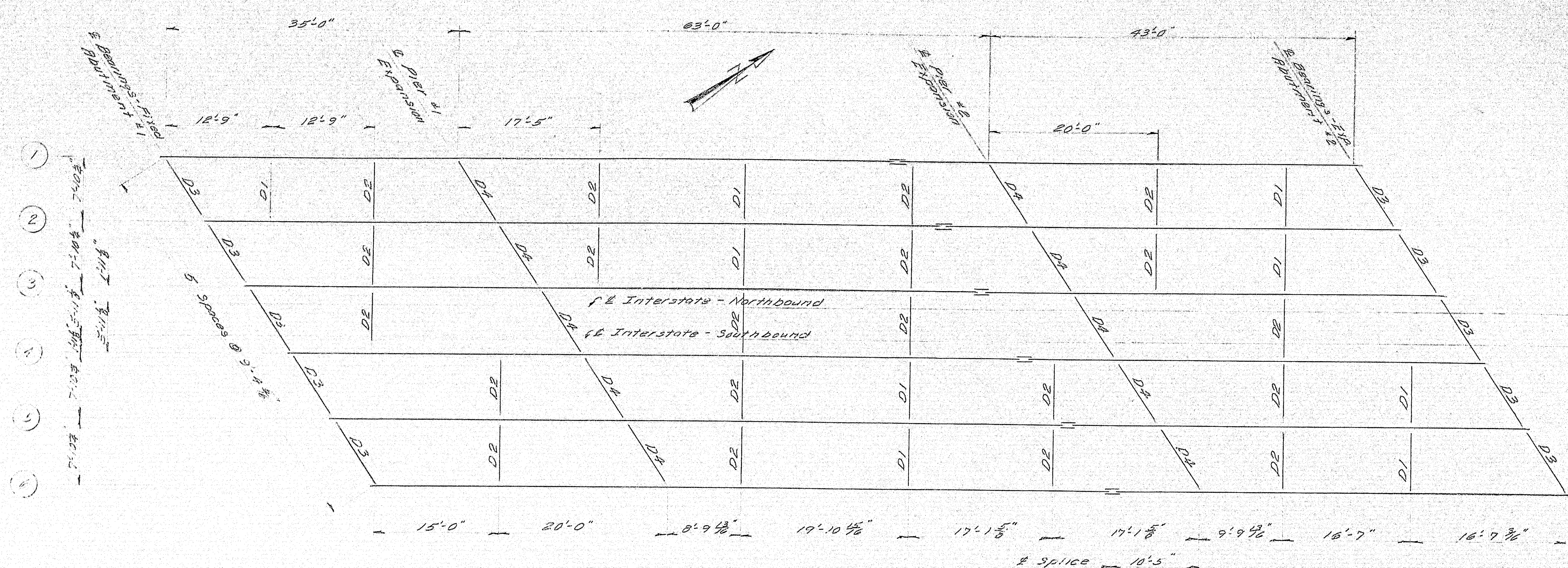
**INTERSTATE 95**  
OVER  
**SOMERSET AVENUE**  
IN THE TOWN OF  
**PITTSFIELD**  
**SOMERSET COUNTY**  
PIERS

SHEET 9 OF 16 AUGUSTA, MAINE FEB. 1963

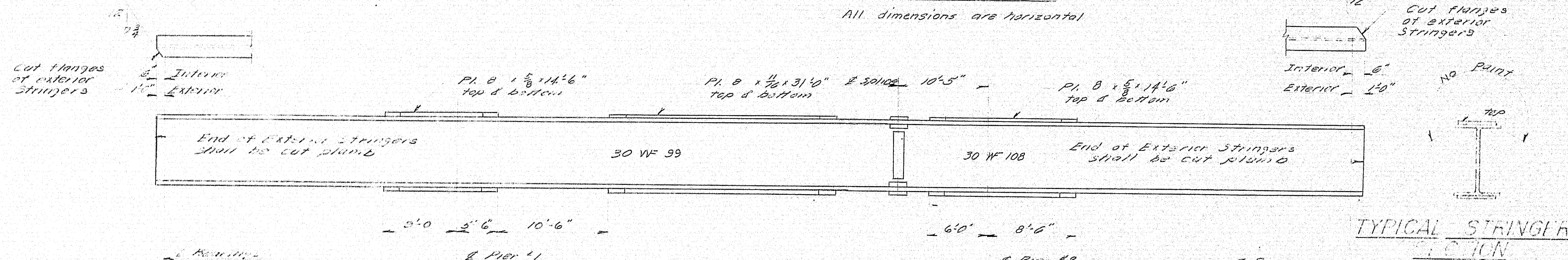
M-1972



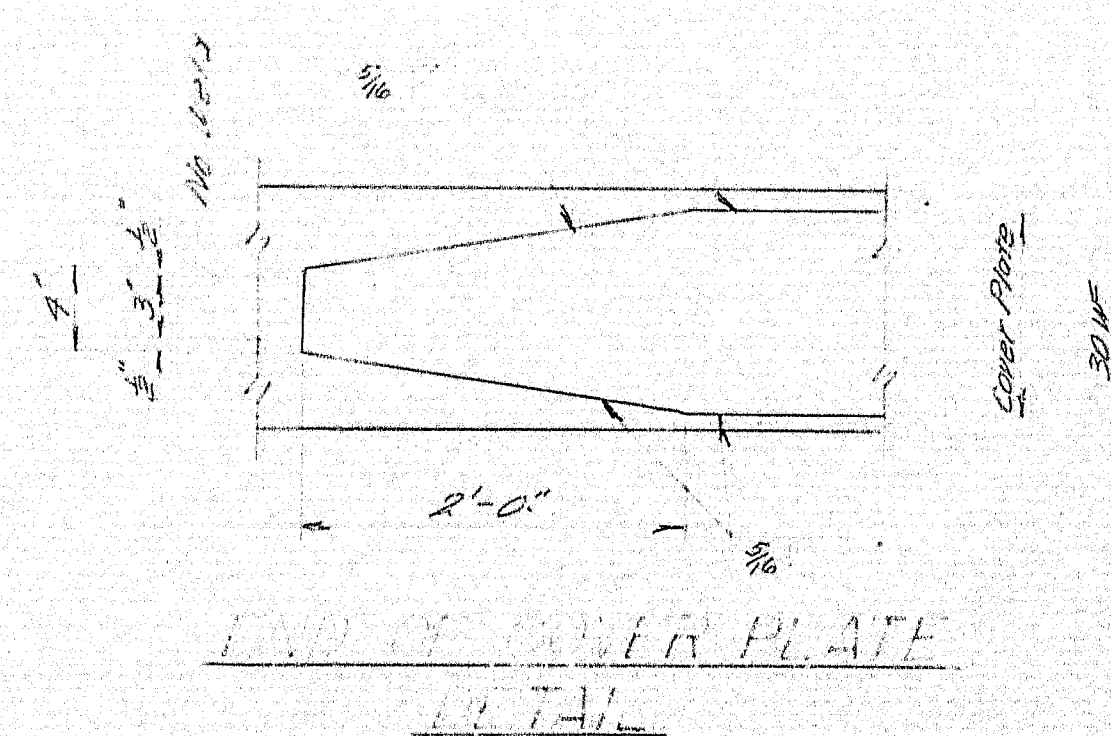




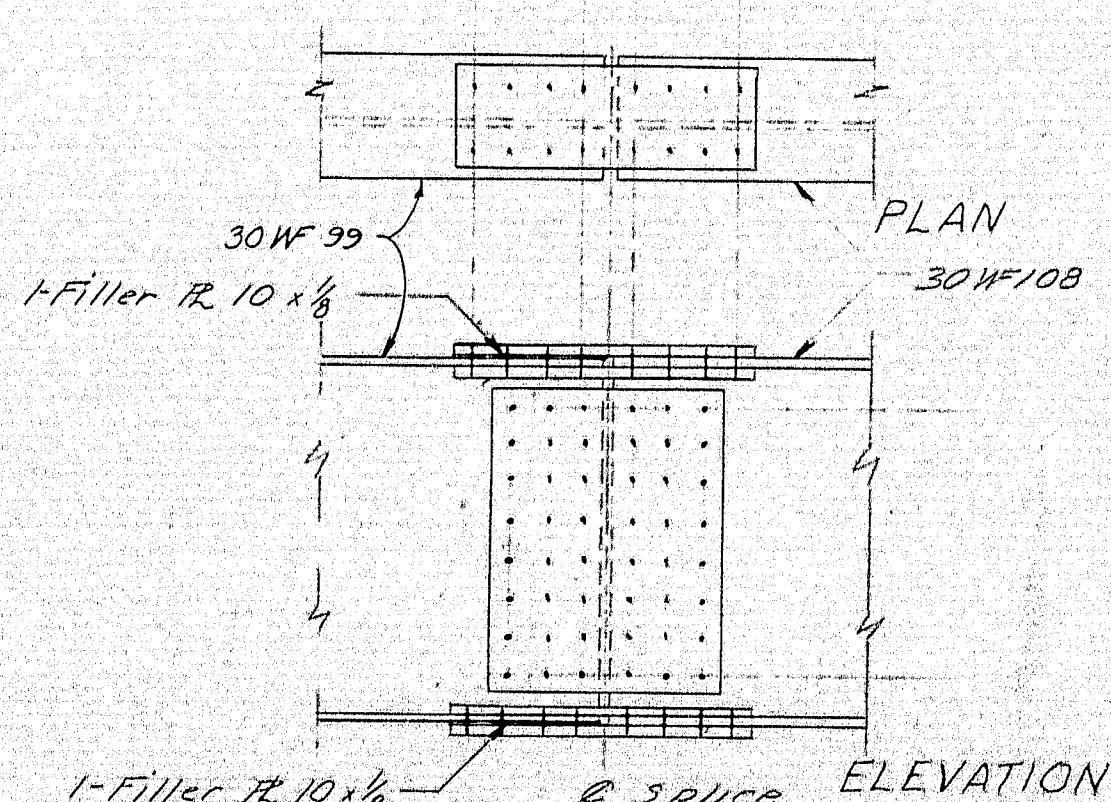
All dimensions are horizontal



STRANGER ELEVATION



END OF COVER PLATE  
ETAL



SPLICE DETAIL

NOTE: For Splice Details not shown, see Standard Details B.D. 103-62

NOTES

4. Stringers, cover plates, and splices shall conform to the latest revision of the Specification A.S.T.M. Designation A 36.
2. All other structural steel shall conform to the latest revision of the Specification A.S.T.M. Designation A 36 or A7.
3. Field connections shall be 3" High Tensile Strength Bolts.
4. The Engineer shall be supplied with a diagram showing match marks of connecting parts assembled in the Shop.
5. No camber required in stringers.

DESIGN: *CDH*  
TRACE: *2240*  
CHECK: *R.H.S.*

BRIDGE NO. -  
SUBJECT  
PLOT -

STATE HIGHWAY COMMISSION  
BRIDGE DIVISION

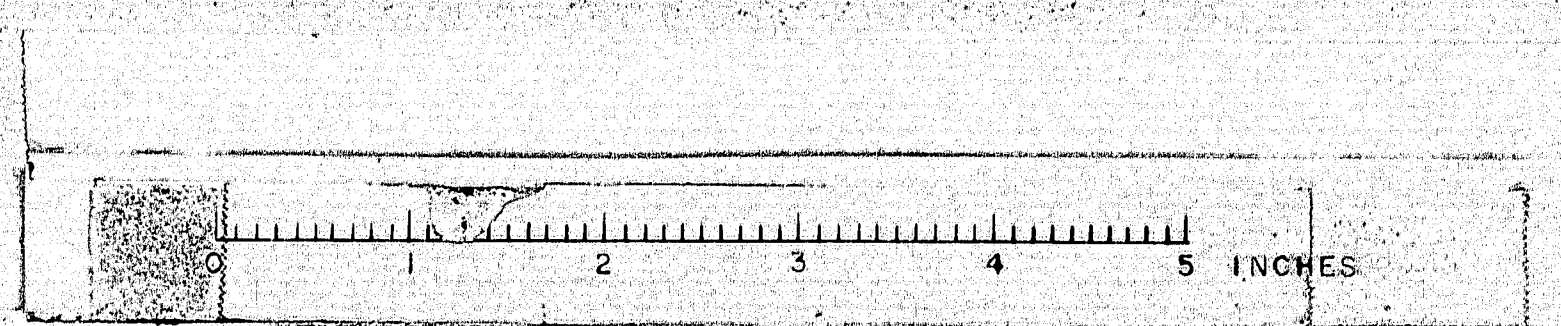
INTERSTATE 95  
OVER

SOMERSET AVENUE  
IN THE TOWN OF  
PITTSFIELD  
SOMERSET COUNTY

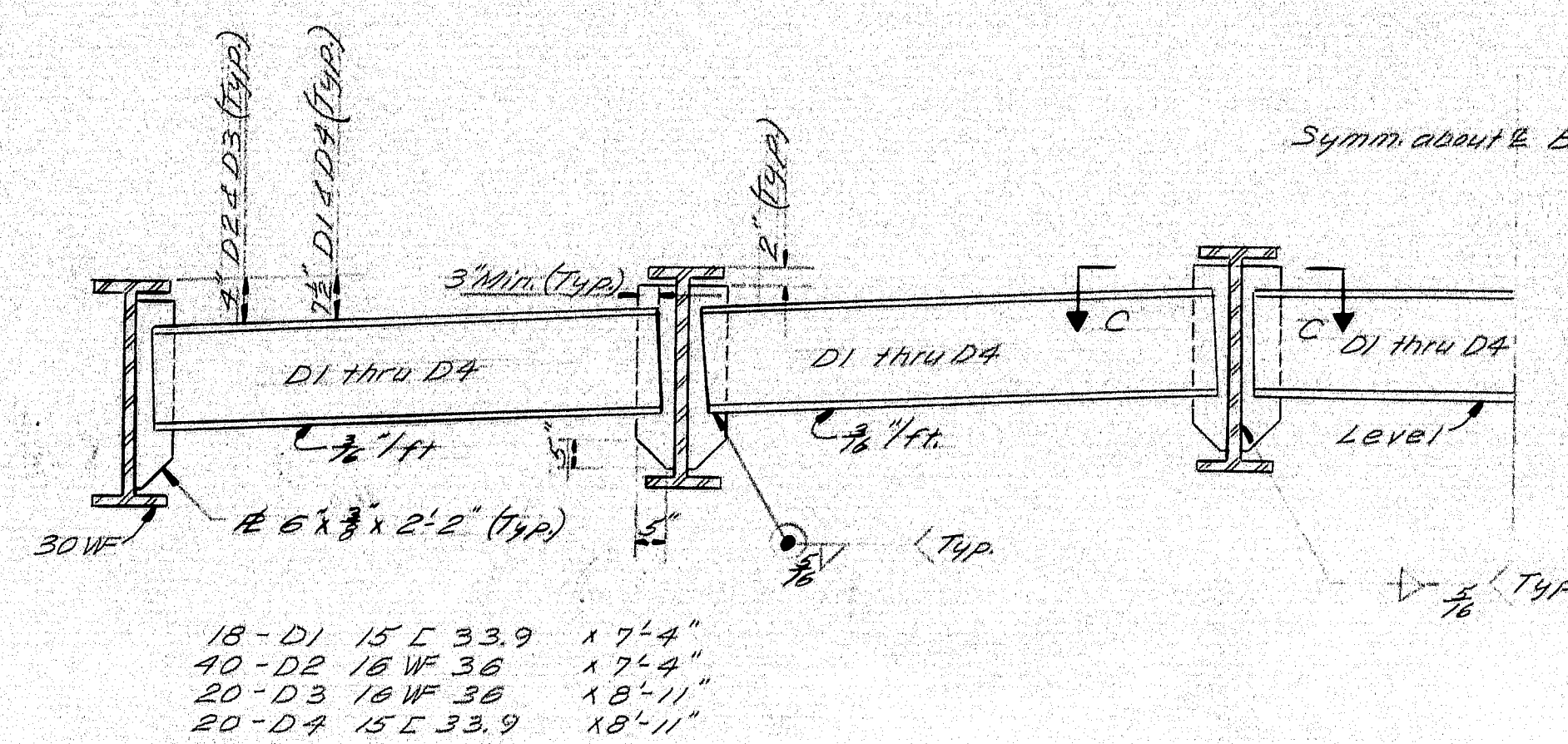
STRUCTURAL STEEL FRAMING PLAN

SHEET 10 OF 16 AUGUSTA, MAINE FEB. 1963

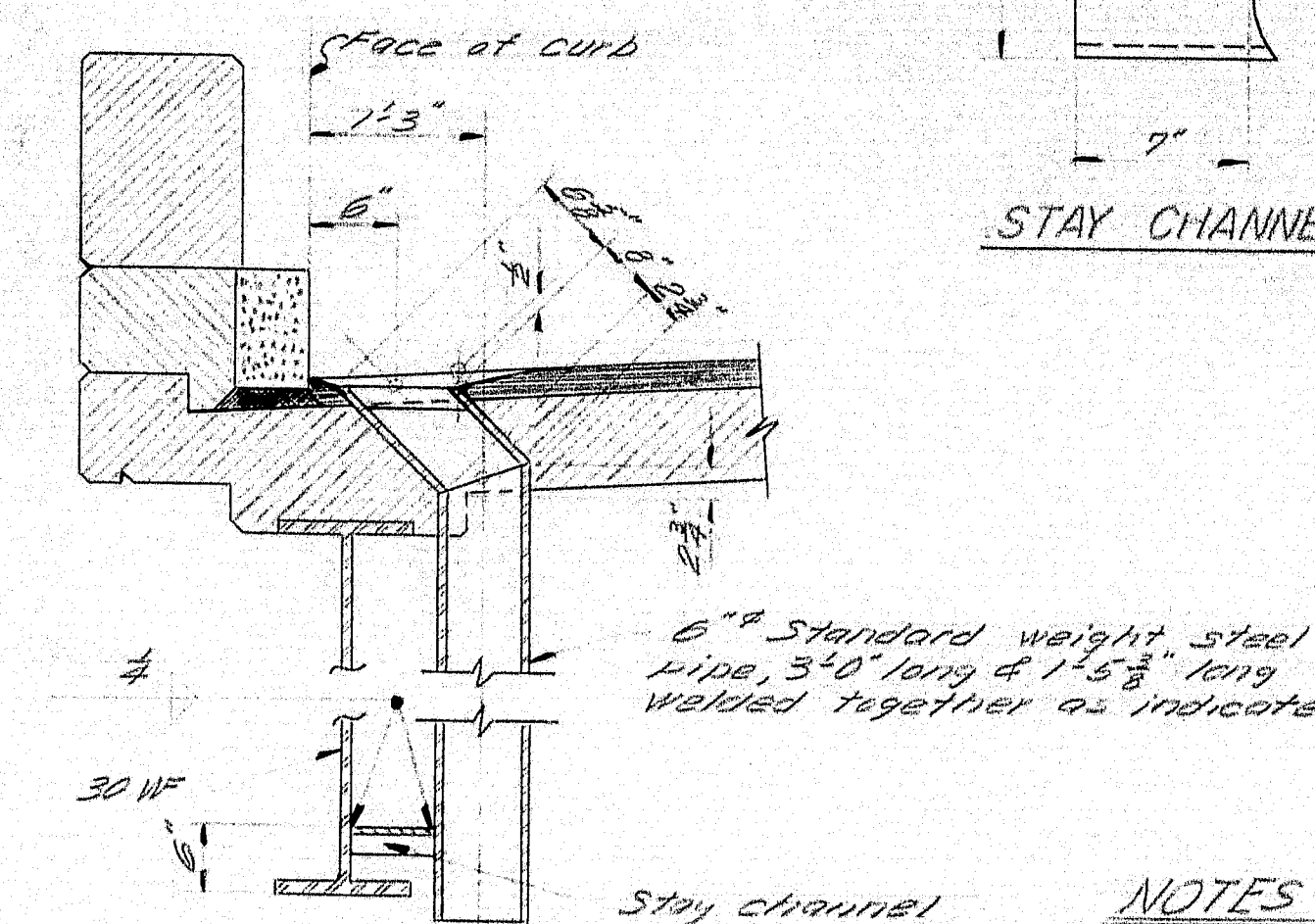
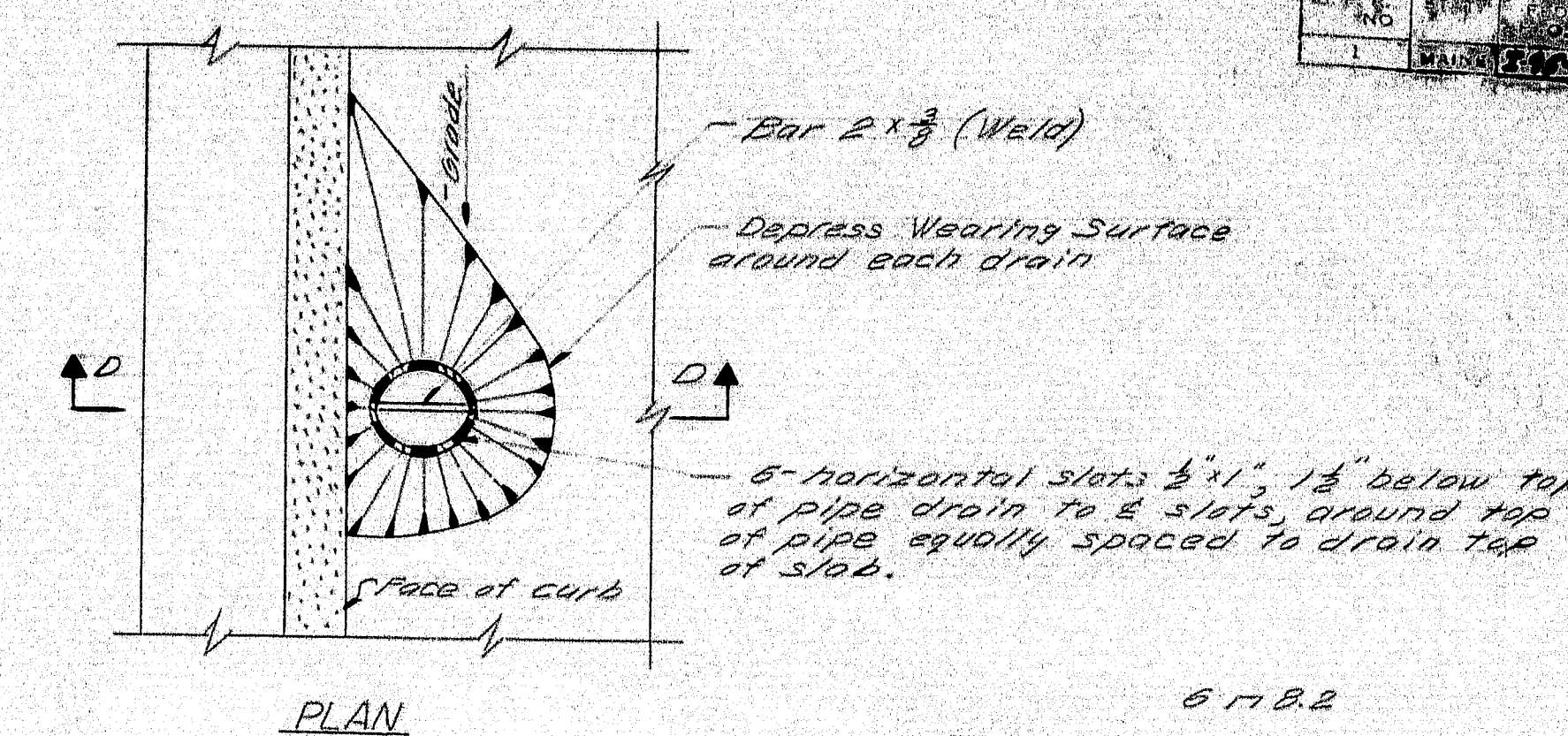
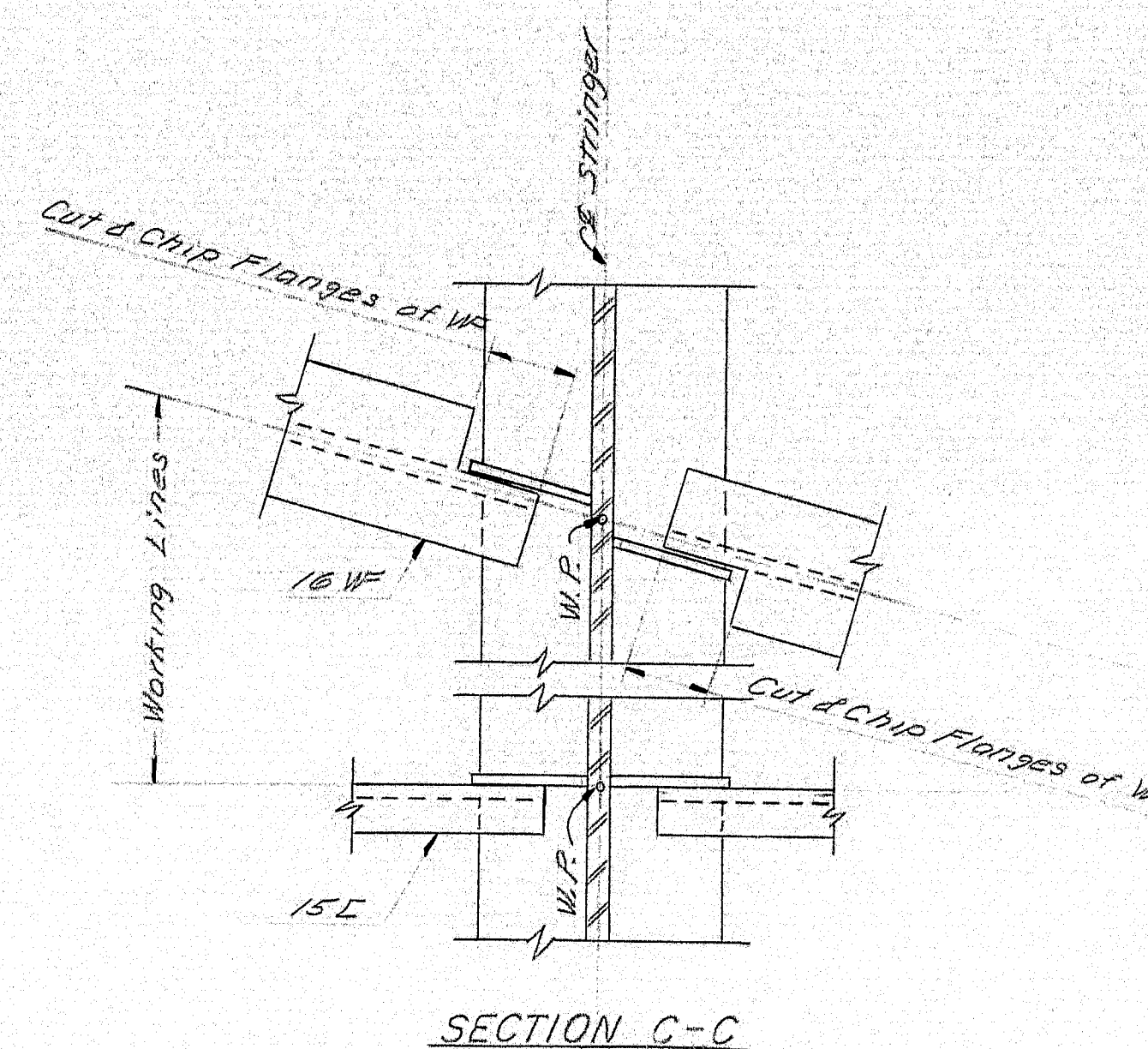
M-1973





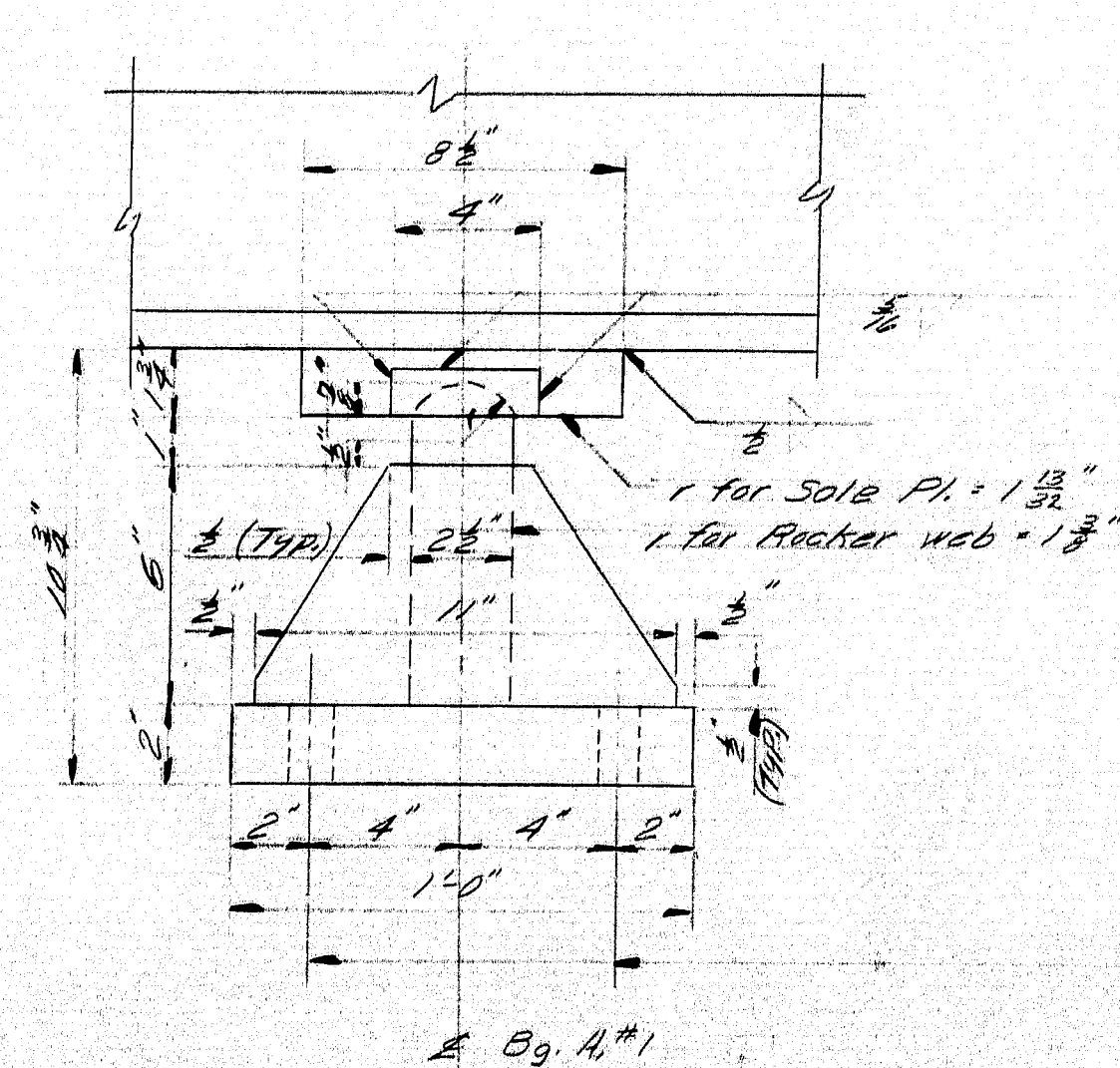


**DIAPHRAGM DETAILS**  
Section Looking Northerly

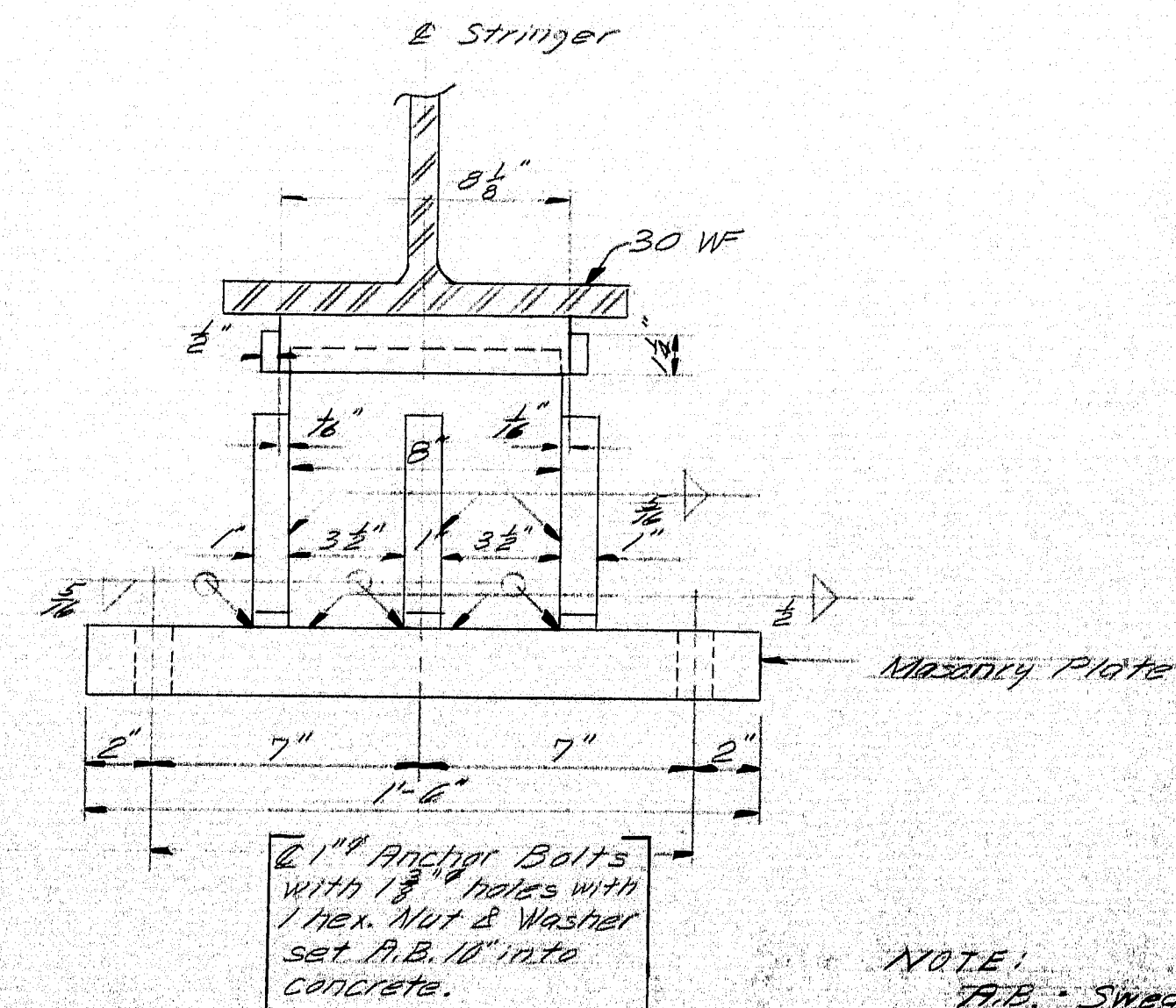


**DRAIN DETAILS**  
16 Req'd

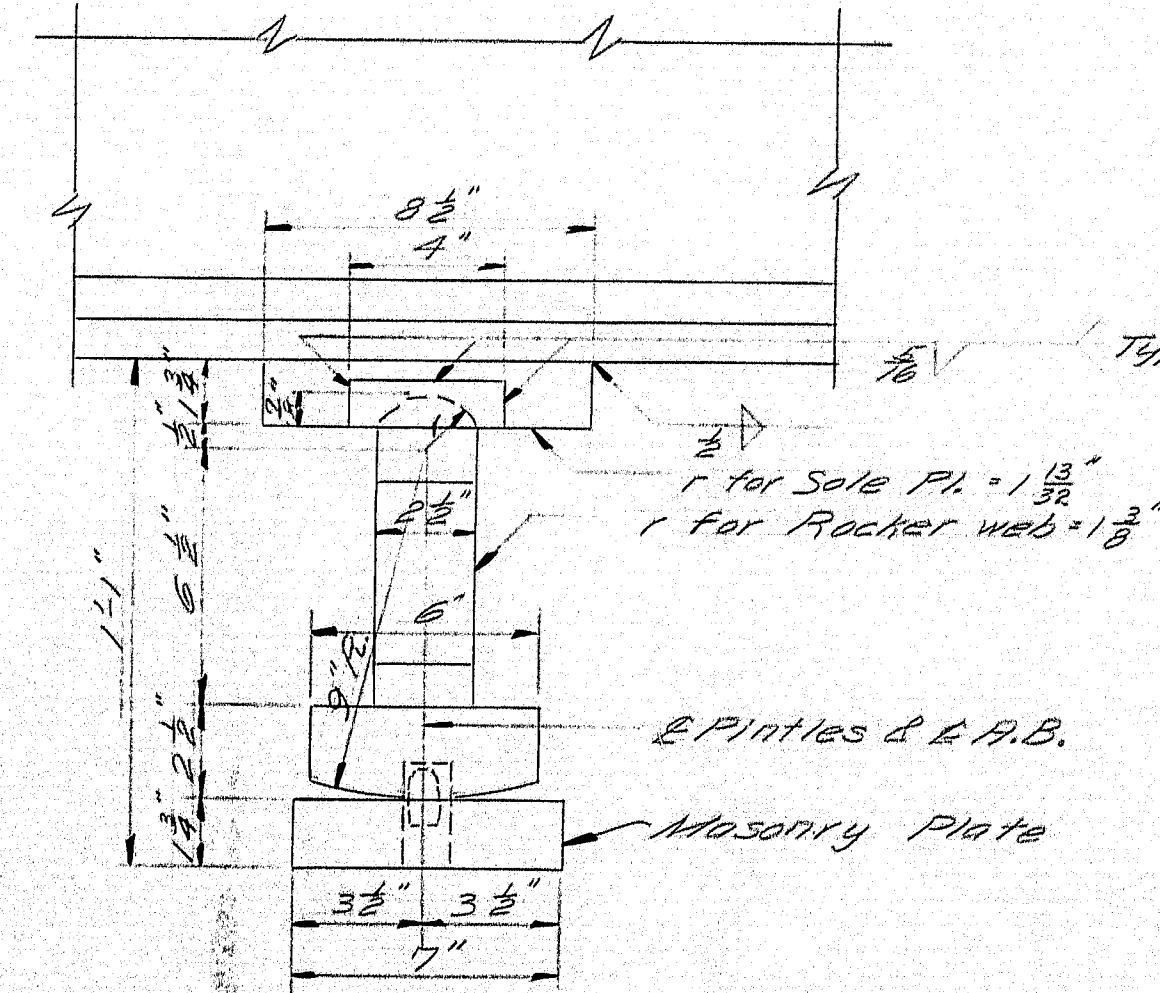
**NOTES**  
1. For location of Drains see Superstructure Sheets.  
2. Payment for Steel Pipe Drains to be included in Lump Sum prices for Items 702-103, 702-104, and 702-105, Structural Steel.



**FIXED BEARINGS**



**EXPANSION BEARINGS**

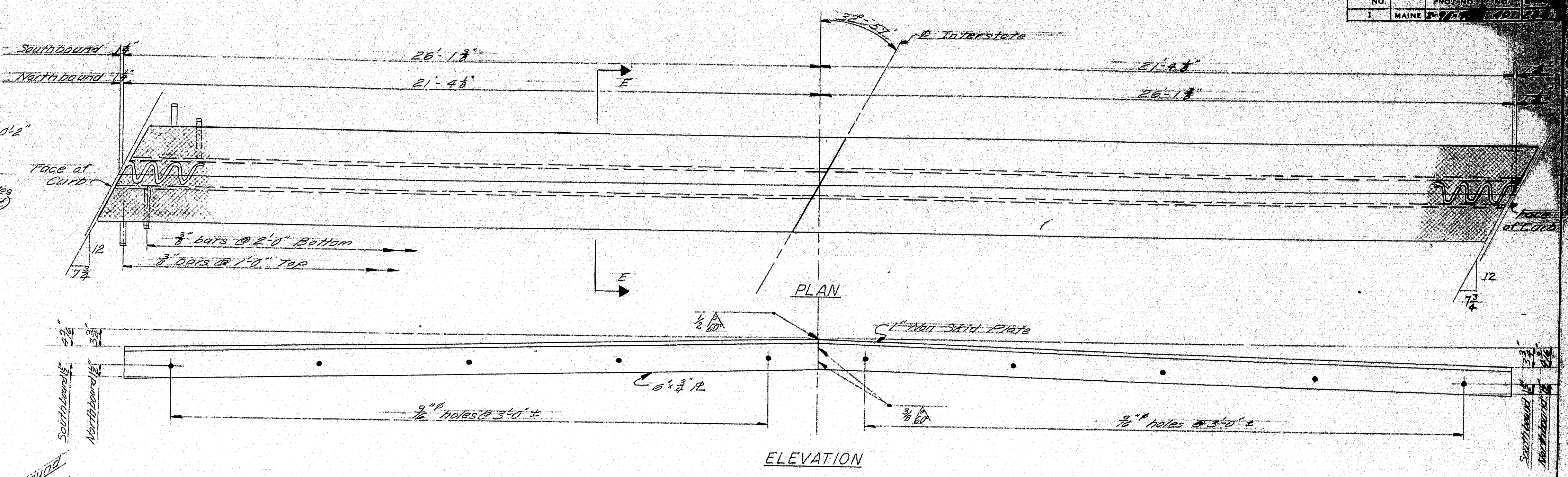
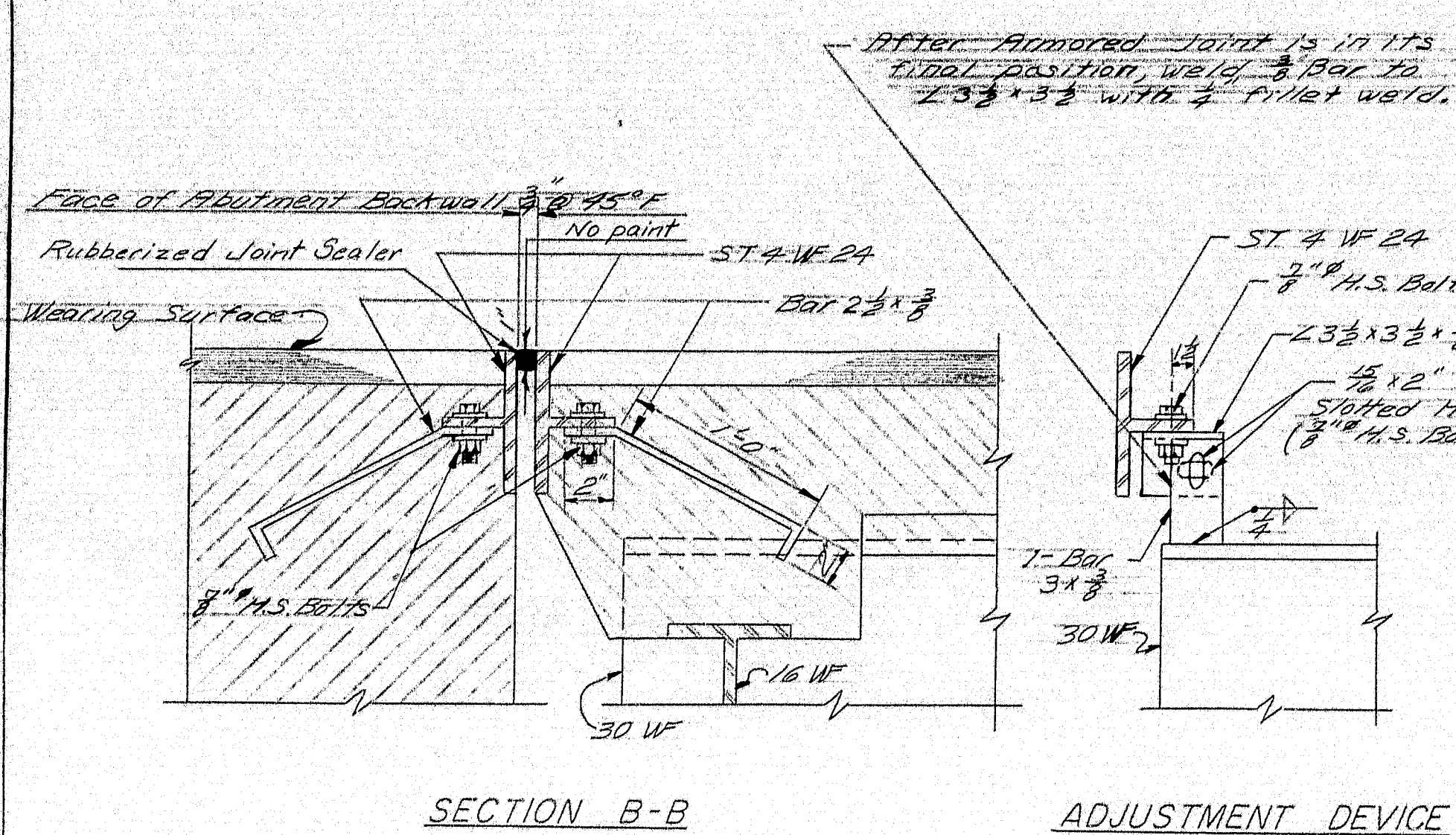


**PINTLE DETAIL**

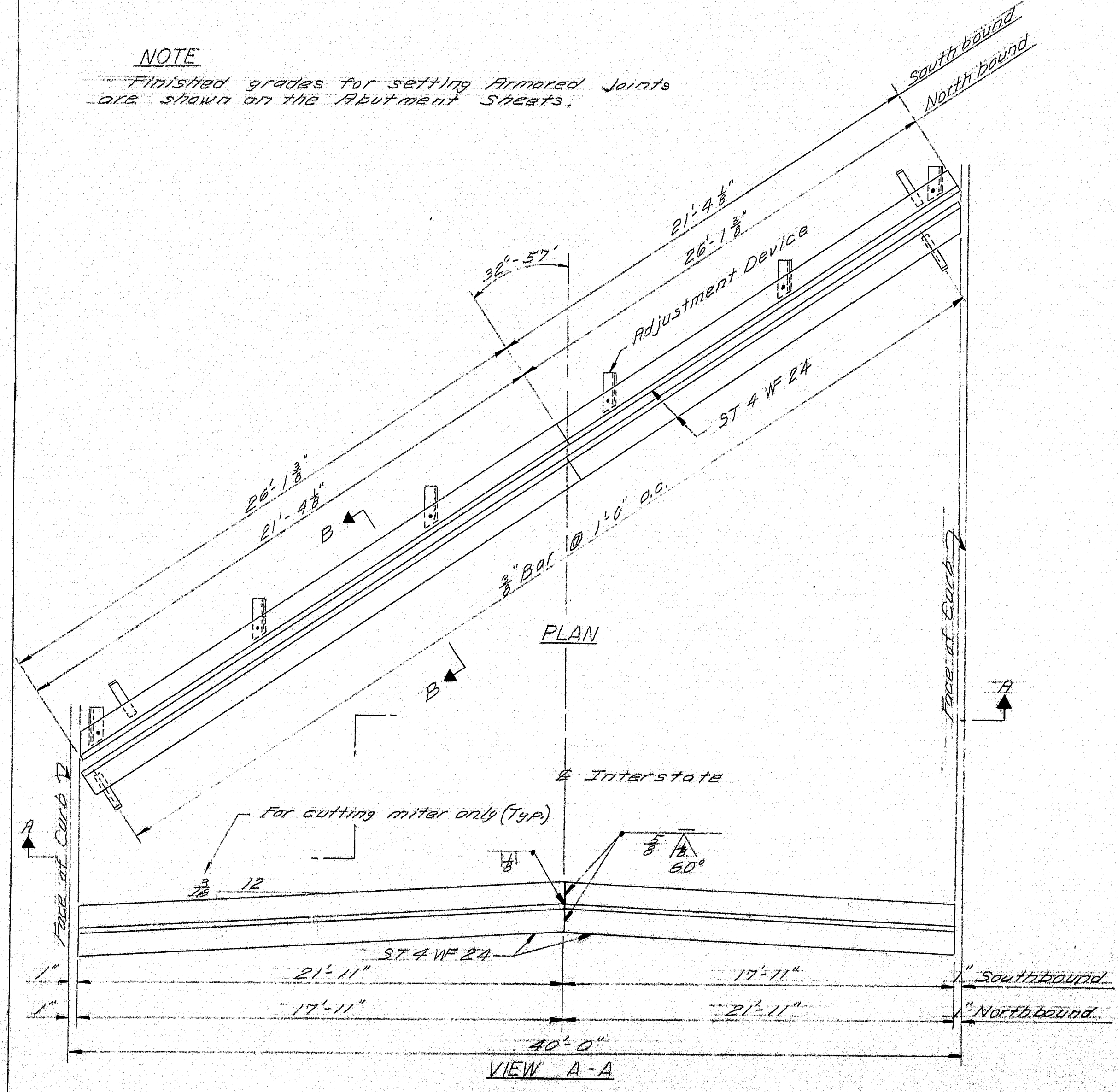
DESIGN - CDH  
TRACE - JHB  
CHECK - EAS.  
STATE HIGHWAY COMMISSION  
BRIDGE DIVISION  
INTERSTATE 95  
OVER  
SOMERSET AVENUE  
IN THE TOWN OF  
PITTSFIELD  
SOMERSET COUNTY  
STRUCTURAL STEEL DETAILS - 1  
SHEET 11 OF 16 AUGUSTA, MAINE FEB. 1962

N-1974



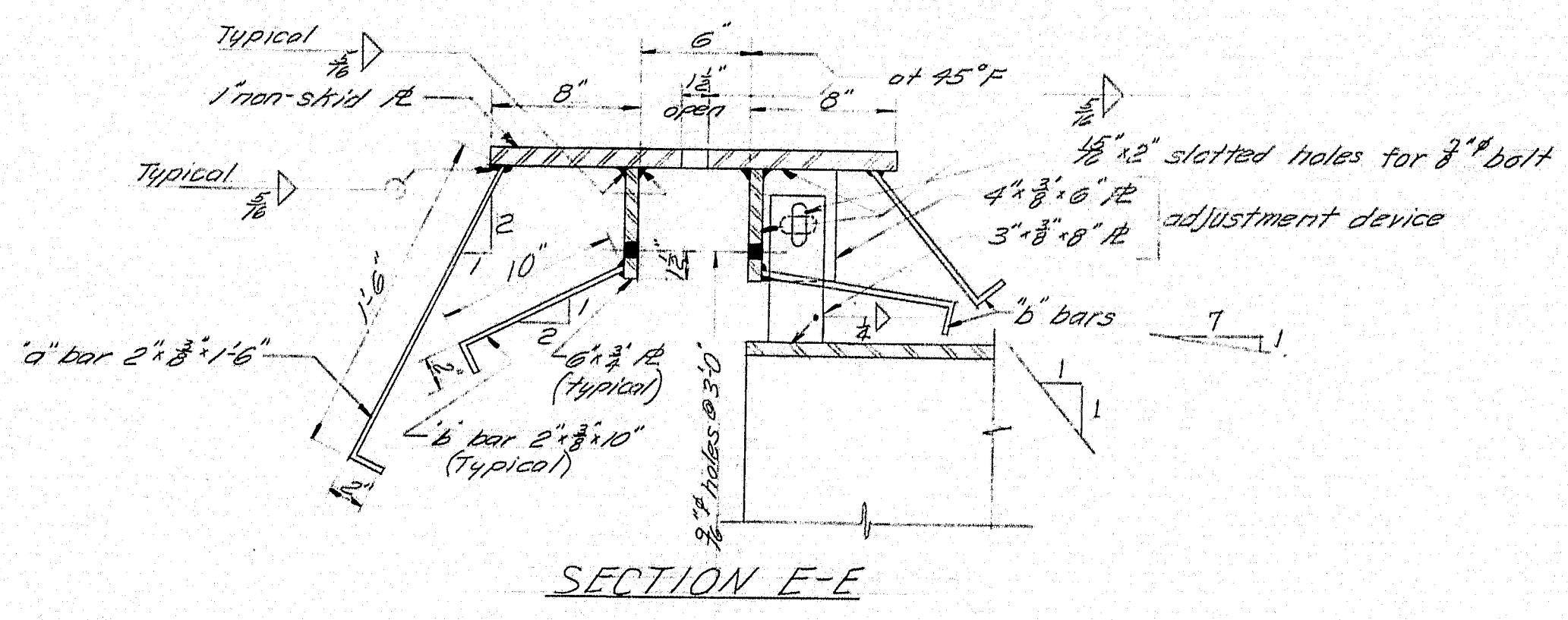


**NOTE**  
 Finished grades for setting Armored Joints are shown on the Abutment Sheets.



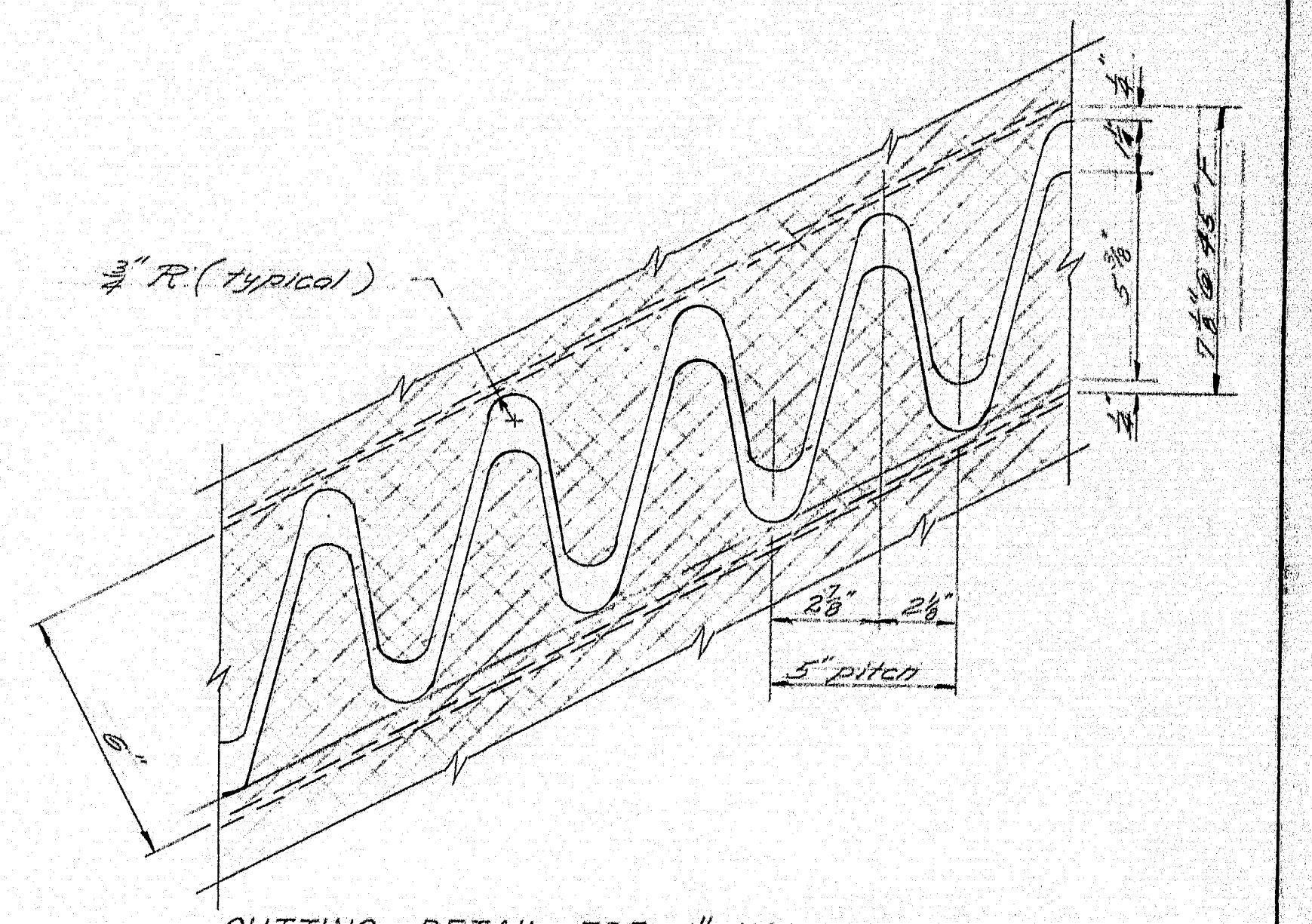
**ARMORED JOINT**

1- Abutment #1 - Northbound  
 1- Abutment #2 - Southbound



**EXPANSION DAM**

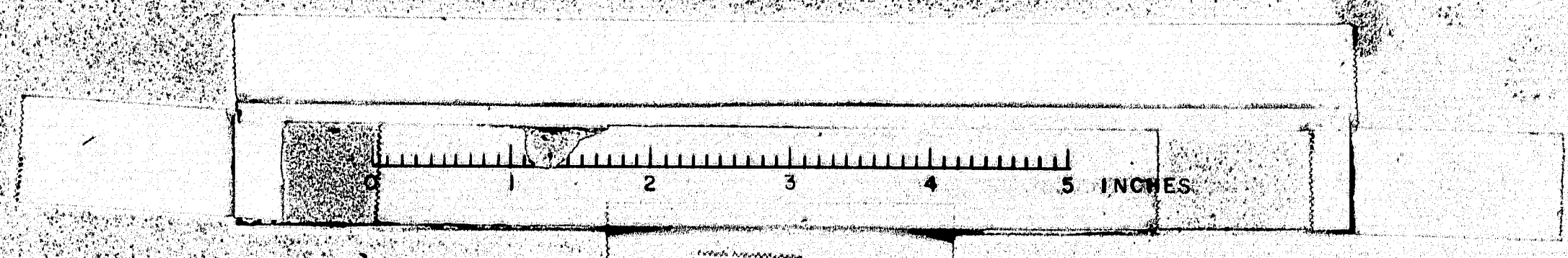
1- Abutment #2 - Northbound  
 1- Abutment #2 - Southbound



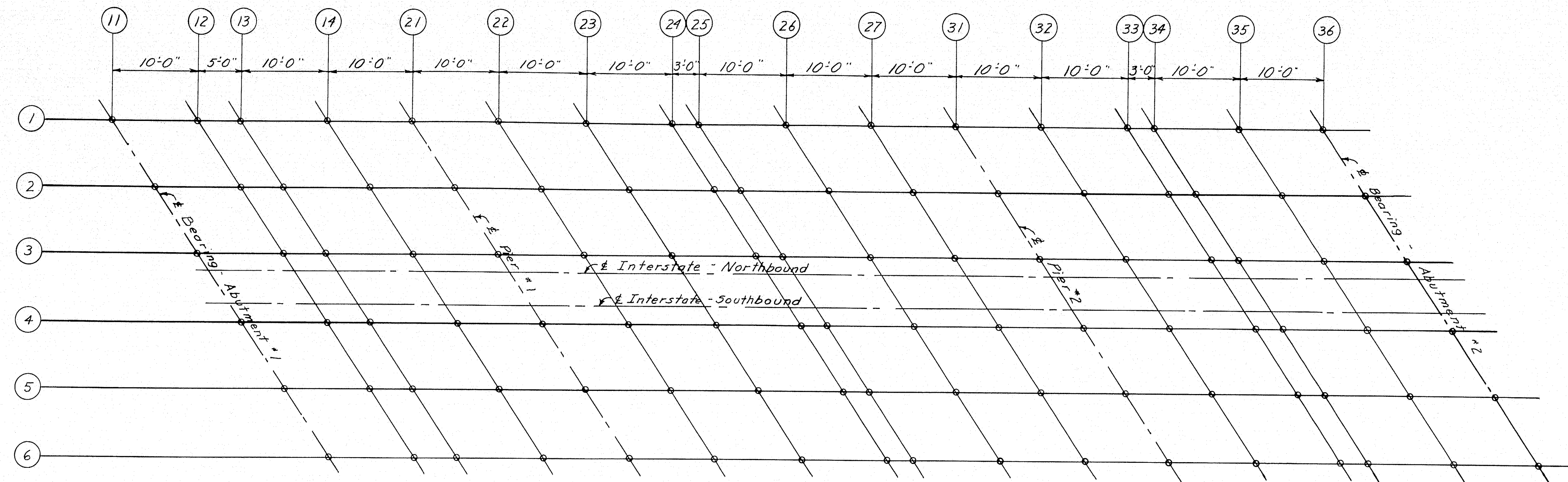
Note: Cut from one plate and match mark

DESIGN - CDM	STATE HIGHWAY COMMISSION
TRACE - JEP	BRIDGE DIVISION
CHECK - E.H.S. & E.E.L.	INTERSTATE 95
	OVER
	SOMERSET AVENUE
	IN THE TOWN OF
	PITTSFIELD
	SOMERSET COUNTY
STRUCTURAL STEEL	DETAILS-2
SHEET 12 OF 16 AUGUSTA, MAINE FEB. 1962	

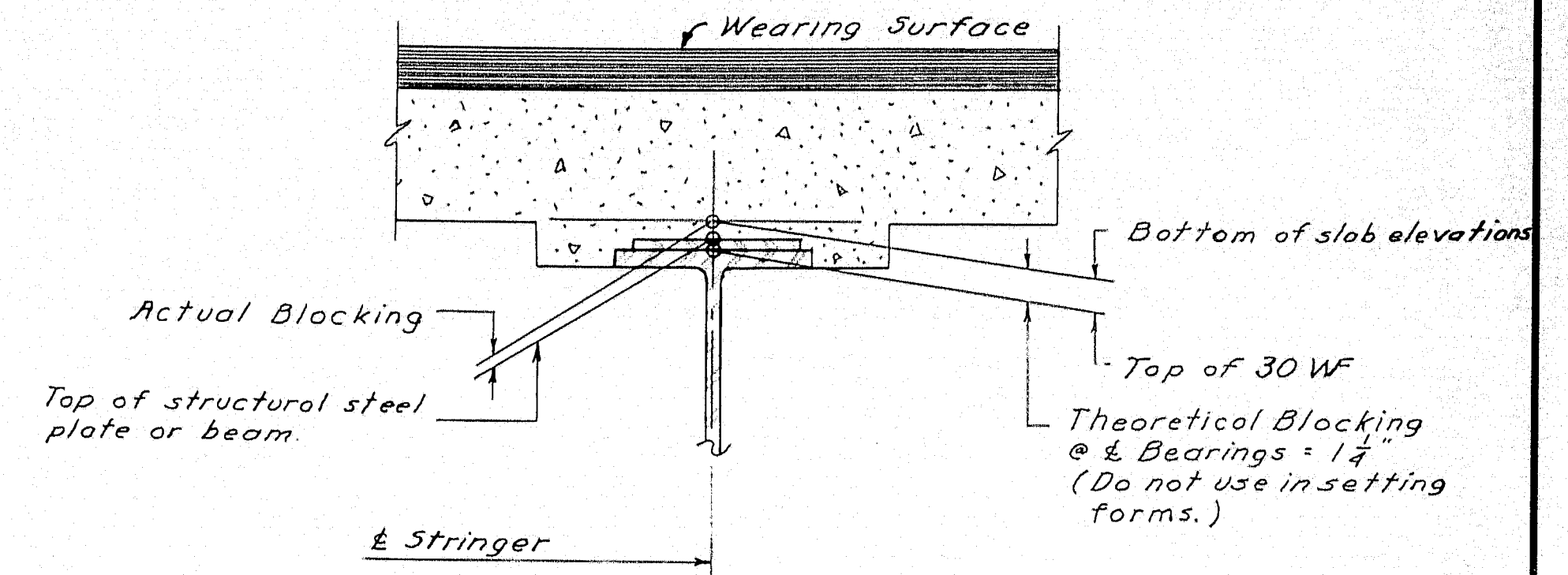
M-1975







**BLOCKING PLAN**

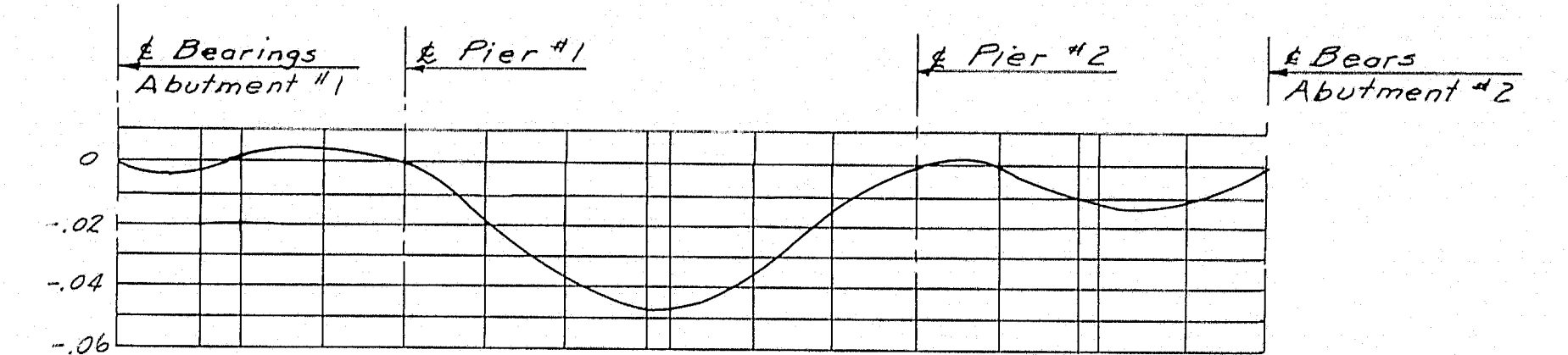


**BLOCKING DETAIL**

**BLOCKING NOTE**

In order that the Wearing Surface may conform to the profile and cross sections shown on these plans, the accompanying tables of "Bottom of Slab Elevations" are given. These elevations, which are computed to compensate for dead load deflections, must be set before slab forms are started.

BOTTOM OF SLAB ELEVATIONS - SOUTHBOUND																	
POINT	11	12	13	14	21	22	23	24	25	26	27	31	32	33	34	35	36
1	221.62	221.97	222.15	222.49	222.85	223.22	223.59	223.95	224.05	224.39	224.72	225.05	225.40	225.76	225.87	226.22	226.56
2	221.94	222.29	222.46	222.81	223.16	223.53	223.90	224.26	224.36	224.70	225.03	225.37	225.72	226.08	226.18	226.53	226.87
3	222.25	222.60	222.78	223.12	223.48	223.85	224.22	224.58	224.68	225.02	225.35	225.68	226.03	226.39	226.50	226.85	227.19
4	222.50	222.85	223.02	223.37	223.72	224.09	224.46	224.82	224.93	225.27	225.60	225.93	226.28	226.64	226.74	227.09	227.43
5	222.54	222.89	223.07	223.41	223.77	224.13	224.50	224.86	224.97	225.31	225.64	225.97	226.32	226.68	226.79	227.14	227.48
6	222.58	222.93	223.11	223.45	223.81	224.18	224.55	224.91	225.01	225.35	225.68	226.01	226.36	226.72	226.83	227.18	227.52

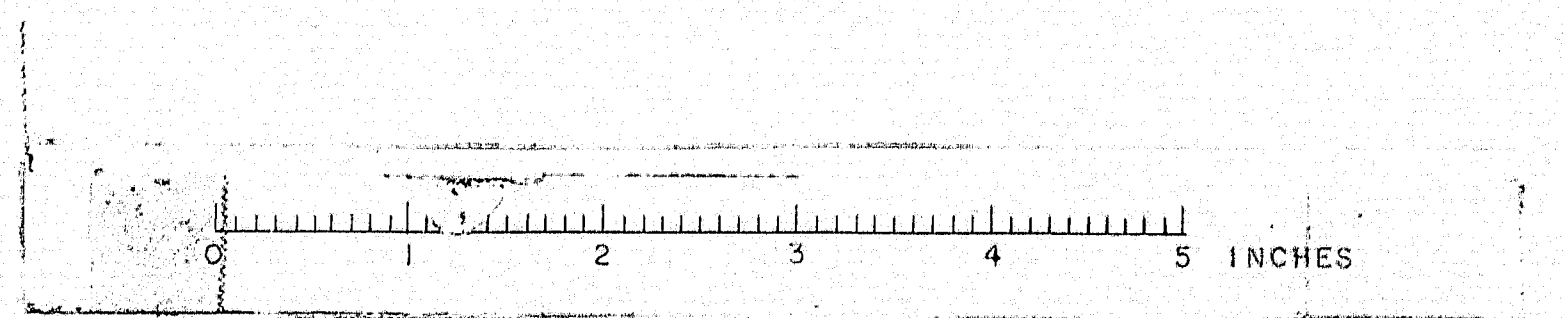


**DEFLECTION CURVE**  
Deflection for Dead Load minus weight of Stringer

BOTTOM OF SLAB ELEVATIONS - NORTHBOUND																	
POINT	11	12	13	14	21	22	23	24	25	26	27	31	32	33	34	35	36
1	227.23	227.53	227.68	227.97	228.28	228.60	228.92	229.23	229.32	229.61	229.89	230.17	230.47	230.78	230.87	231.17	231.46
2	227.52	227.82	227.97	228.26	228.57	228.89	229.21	229.52	229.61	229.89	230.17	230.46	230.76	231.07	231.16	231.46	231.75
3	227.81	228.11	228.26	228.55	228.86	229.18	229.50	229.81	229.90	230.18	230.46	230.75	231.05	231.36	231.45	231.75	232.04
4	227.89	228.19	228.34	228.64	228.94	229.26	229.58	229.89	229.98	230.27	230.55	230.83	231.13	231.44	231.53	231.83	232.12
5	227.91	228.21	228.36	228.65	228.96	229.28	229.60	229.91	230.00	230.28	230.55	230.85	231.15	231.46	231.55	231.85	232.14
6	227.92	228.22	228.37	228.67	228.97	229.29	229.61	229.92	230.01	230.30	230.56	230.86	231.16	231.47	231.56	231.86	232.15

DESIGN - C. G. W.	BRIDGE NO.
TRACE - B. S. W.	SURVEY -
CHECK - B. A. S. & E. E. L.	PLOT -
STATE HIGHWAY COMMISSION BRIDGE DIVISION	
INTERSTATE 95 OVER SOMERSET AVENUE IN THE TOWN OF PITTSFIELD SOMERSET COUNTY BLOCKING DETAILS	
SHEET 13 OF 16 AUGUSTA, MAINE FEB. 1963	

M-1976









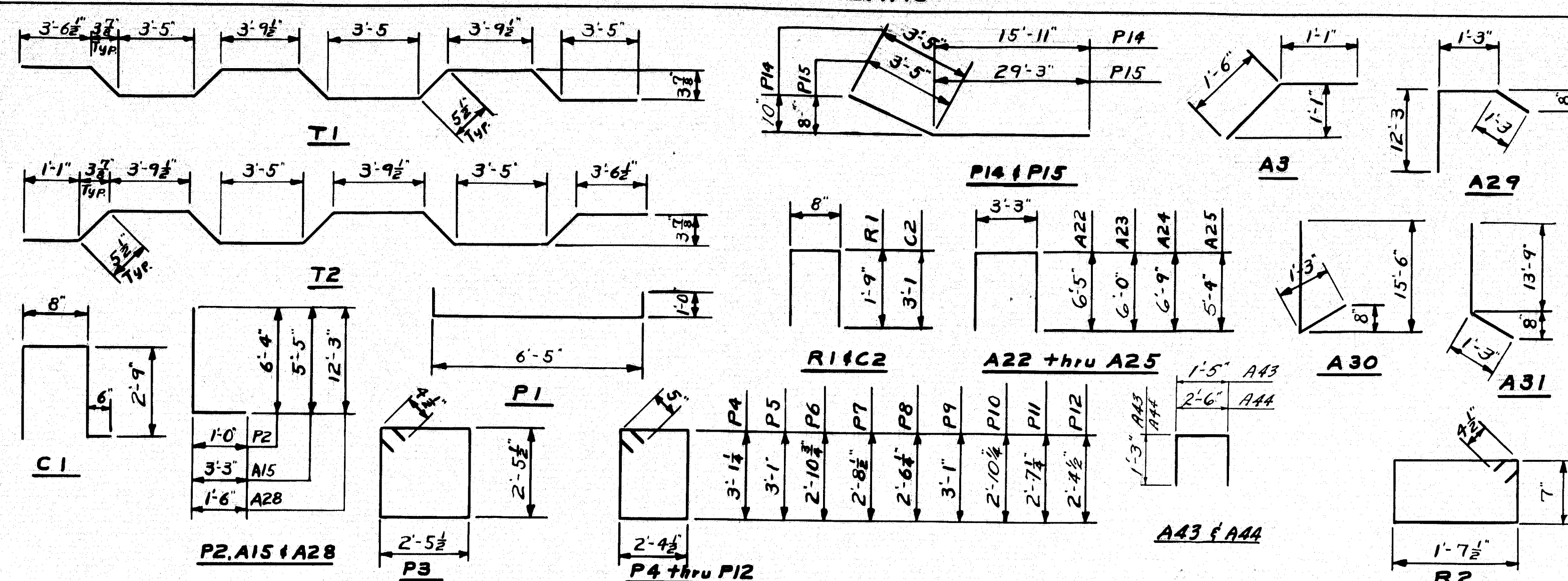




# REINFORCING STEEL SCHEDULE

D. P. B. NO. 1  
STATE MAINE  
PROJECT NUMBER I-95-7 (B2)  
SHEET NO. 44  
TOTAL SHEETS 226

## BENT BARS



## STRAIGHT BARS

MARK	SIZE	NO.	LENGTH	LOCATION	MARK	SIZE	NO.	LENGTH	LOCATION
F1	#5	560	21'-10"	Superstructure - slab - trans.	P13	#6	32	25'-6"	Pier cap
F2		16	2'-11"		P16	#10	40	25'-11"	
F3			4'-2"		P17	#7	32	7'-0"	
F4			5'-5"		P18	#7	32	6'-6"	
F5			6'-9"		P19	#9	36	16'-0"	Pier columns
F6			8'-0"		P20		24	17'-0"	
F7			9'-4"		P21		24	18'-0"	
F8			10'-7"		P22		12	18'-9"	
F9			11'-11"		P23		24	17'-9"	
F10			13'-2"		P24		24	17'-3"	
F11			14'-5"		P25		12	22'-9"	
F12			15'-9"		P26		12	20'-0"	
F13			17'-0"		P27		12	19'-3"	
F14			18'-4"		P28		12	18'-6"	
F15			19'-7"		B1	#5	280	18'-0"	Superstructure slab - trans
F16			20'-11"		B2	#5	280	25'-8"	do
F17			22'-2"						
F18			23'-5"						
F19			24'-9"						
F20			26'-0"						
F21			27'-4"						
F22			28'-7"						
F23			29'-11"						
F24			31'-2"						
F25			32'-5"						
F26			33'-9"						
F27			35'-0"						
F28			36'-4"						
F29			37'-7"						
F30			38'-11"						
F31		16	40'-0"						
S1		44	16'-0"	- long.					
L1		616	37'-0"	- over pier					
C19	#4	8	12'-4"	Superstructure parapets					
C3			13'-10"						
C4			8'-8"						
C5			17'-7"						
C6			21'-2"						
C7			14'-11"						
C8			15'-6"						
C9		16	20'-4"						
C10		8	7'-9"						
C11			21'-2"						
C12			15'-2"						
C13			7'-10"						
C14			23'-6"						
C15			17'-4"						
C16			8'-11"						
C17			20'-8"						
C18			13'-2"						

NOTES:  
1. All bars shall be intermediate grade steel.  
2. All dimensions are to the center line of bars.

DESIGN - C.D.H.  
TRACE - L.E.O.  
CHECK - P.A.S. & E.E.L.  
BRIDGE NO. 95  
SURVEY - PLOT -  
STATE HIGHWAY COMMISSION  
BRIDGE DIVISION  
INTERSTATE 95  
OVER  
SOMERSET AVENUE  
IN THE TOWN OF  
PITTSFIELD  
SOMERSET COUNTY  
REINFORCING STEEL SCHEDULE  
SHEET 16 OF 16 AUGUSTA, MAINE FEB 1963

